

To: Councillor Ayub (Chair)
Councillors Lanzoni, Barnett-Ward, Cross,
Ennis, Gittings, Griffith, Goss, Hacker,
Hornsby-Smith, Keeping, Kitchingham,
McCann, Moore, Page, R Singh and White

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3 November 2023

Your contact is: **Richard Woodford - Committee Services**

NOTICE OF MEETING - TRAFFIC MANAGEMENT SUB-COMMITTEE 13 NOVEMBER 2023

A meeting of the Traffic Management Sub-Committee will be held on Monday, 13 November 2023 at 6.30 pm in the Council Chamber, Civic Offices, Reading. The Agenda for the meeting is set out below.

<u>ACTION</u>	<u>WARDS AFFECTED</u>	<u>Page No</u>
1. DECLARATIONS OF INTEREST		
2. MINUTES OF PREVIOUS MEETING		5 - 14
3. QUESTIONS FROM MEMBERS OF THE PUBLIC AND COUNCILLORS		

Questions submitted pursuant to Standing Order 36 in relation to matters falling within the Sub-Committee's Powers & Duties which have been submitted in writing and received by the Head of Legal & Democratic Services no later than four clear working days before the meeting.

4. **PETITIONS**

To receive petitions on traffic management matters submitted in accordance with the Sub-Committee's Terms of Reference.

4 (a)	Petition - Hemdean House School	CAVERSHAM	15 - 18
	To report to the Sub-Committee the receipt of a petition from Hemdean House School.		

4 (b)	Petition - Holmes Road Traffic Plug	PARK	19 - 26
	To report to the Sub-Committee the receipt of a petition requesting the Council to install a one-way traffic plug on Holmes Road, at its junction with Wokingham Road, to tackle reported issues of speeding and through traffic.		

5.	PARKING RESTRICTIONS AT FORMER RETAIL PARK EXIT CHATHAM STREET	ABBEY	27 - 34
	A report on traffic management measures associated with the development at the former Wickes site on Weldale Street/Chatham Street and seeking approval to carry out Statutory Consultation on the introduction of loading restrictions within a vehicular exit onto Chatham Street related to the retail park that was previously occupied by Wickes and Iceland.		

6.	REQUESTS FOR TRAFFIC MANAGEMENT MEASURES	BOROUGHWIDE	35 - 100
	A report informing the Sub-Committee of requests for Traffic Management Measures that have been raised with officers and are measures that have either been previously reported, or those that would not typically be addressed in other programmes, where funding is yet to be identified.		

7. EXCLUSION OF PRESS AND PUBLIC

The following motion will be moved by the Chair:

“That, pursuant to Section 100A of the Local Government Act 1972 (as amended) members of the press and public be excluded during consideration of the following item on the agenda, as it is likely that there would be disclosure of exempt information as defined in the relevant Paragraphs of Part 1 of Schedule 12A of that Act”

8.	APPLICATIONS FOR DISCRETIONARY PARKING PERMITS	BOROUGHWIDE	101 - 296
	To consider appeals against the refusal of applications for the issue of discretionary parking permits.		

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Present: Councillors Ayub (Chair for all items except item 19), Barnett-Ward, Cross, Ennis, Gittings, Goss, Griffith, Hacker, Hornsby-Smith, Keeping, Lanzoni (Vice Chair in the Chair for item 19), McCann, Moore, Page, Singh and White.

Apologies: Councillor Kitchingham.

15. DECLARATIONS OF INTEREST

Councillor Ayub declared an interest in item 19 on the grounds that he owned a hackney carriage.

16. MINUTES

The Minutes of the meeting of 14 June 2023 were confirmed as a correct record and signed by the Chair.

17. QUESTIONS

A question on the following matter was submitted, and answered by the Lead Councillor for Climate Strategy and Transport on behalf of the Chair:

Questioner	Subject
Councillor White	Improving Road Safety Around Schools

(The full text of the question and reply was made available on the Reading Borough Council website).

18. PETITIONS

(a) Petition – Speeding on Francis Street

The Sub-Committee considered a report on the receipt of a petition, requesting the Council to address alleged speeding and traffic volumes along Francis Street. A map showing Francis Street was attached to the report at Appendix 1.

The report stated that on 8 August 2023 a petition had been submitted to the Council that had contained 32 signatures. The petition read as follows:

“Reading Council, non resident car owner, delivery vans, and bin vans often drive too fast on Francis Street. Residents’ cars are parked on both sides of the street. With an increase of “through” traffic and the number of wide vans, residents’ cars are regularly scratched, dented and damaged often with no recourse or named parties given for insurance claims.

We would like to park our cars without worrying that they will be damaged and want to avoid a pedestrian or cyclist being hurt. Reading Council; the residents agree that we need to tackle speeding on Francis Street with anti-speeding measures and investigate other ways to cut the volume of traffic on the street.”

The report explained that Francis Street was a one-way street that ran west to east between Elgar Road and Sherman Road and that within the previous five years of Police supplied casualty data up to 1 April 2023 there had been no recorded incidents on the Street.

The Requests for Traffic Management Measures report that was submitted to the Sub-Committee twice annually contained an entry for requested traffic calmed 20mph restrictions on Alpine Street, Francis Street and Edgehill Road specifically. The list had been submitted to the Sub-Committee meeting on 2 March 2023, Minute 54 refers, and it was intended to submit an update to the next meeting on 13 November 2023.

The report stated that there was currently no allocated funding for the development and delivery of a 20mph zone on Francis Street and officers were aware of the desire for similar treatment in the surrounding area. Officers acknowledged the concerns that had been raised by residents both in the petition and with prior direct contact and 20mph appeared to be appropriate for the nature of the street. Therefore, the report proposed that the entry in the Requests for Traffic Management Measures Report should be updated to reflect the receipt of the petition, with an expectation that scheme development would commence once funding had been identified. The application of traffic calming features, such as speed humps, required statutory consultation and could be locally controversial, but would likely also deter motorists from using these streets as short-cuts, and would additionally address the concerns that had been raised in relation to current traffic volumes.

At the invitation of the Chair Ian Fullbrook, addressed the Sub-Committee on behalf of the petitioners.

Resolved –

- (1) That the report be noted;**
- (2) That the existing entry on the ‘Requests for Traffic Management Measures’ report be updated to reflect the receipt of the petition be agreed;**
- (3) That the lead petitioner be informed of the decisions of the Sub-Committee, following publication of the agreed minutes of the meeting;**
- (4) That no public inquiry be held into the proposals.**

(b) Petition – Improving Safety at Kenilworth Avenue and Monks Way Entrances

The Sub-Committee considered a report on the receipt of a petition, requesting the Council to improve the safety of the entrance of Kenilworth Avenue and Monks Way, with requests for raised pedestrian crossings and waiting restrictions. A copy of the petition was attached to the report at Appendix 1.

The report explained that on 22 August 2023 a petition had been submitted to the Council from the Southcote Park Estate Committee that requested three changes:

- The implementation of double-yellow lines around the eastern corner of Kenilworth Avenue, continuing eastbound along Southcote Lane. This was being requested to improve visibility for motorists exiting Kenilworth Avenue onto Southcote Lane, which was currently being compromised by on-street parking;

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- Raised table/bump across Kenilworth Avenue at its junction with Southcote Lane. This was being requested to align with other junctions along Southcote Lane that had had this treatment. It was anticipated to help reinforce pedestrian priorities at the junction which the recent Highway Code changes had introduced;
- Raised table/bump across Monks Way, at its junction with Southcote Lane.

The report stated that beyond the junctions with Southcote Lane these sections of Kenilworth Avenue and Monks Way were not adopted Highway. However, the Highway adoption of Southcote Lane extended into the junctions, so any feature/design would need to be feasible within that adopted section. Tables of this nature should be set back from the junction give-way lines to reduce the risk of vehicle destabilisation when turning and to ensure that the give-way markings were prominent. The table would have entry/exit ramps of a prescribed gradient and a minimum table width of 1.5m should be provided for pedestrians. It was expected that the current extent of adopted land would not fully accommodate this, so development of the scheme would likely require further land adoption, albeit to a relatively small extent to make the scheme feasible. This would allow necessary realignment of the approaching footways and an offset of the table from the junction. Other raised tables along Southcote Lane were set back from the junction and occupied approximately five to five and a half metres between the junction and the end of the furthest ramp. These were relatively wide tables which could be reduced for application at Kenilworth Avenue and Monks Way, but not to the three metres adopted length that had been suggested within the petition.

Surface water drainage would be a consideration of the design and if additional gullies were required this might also necessitate additional potential adoption of land to enable feasibility of the installation.

The report stated that within the previous five year period of Police supplied casualty data up to 1 April 2023, there had been no recorded incidents either at the junction of Kenilworth Avenue or Monks Way.

The report explained that the request for double-yellow lines had already been captured on the list of new requests proposed for the 2023B Waiting Restriction Review Programme, see Minute 21 below, and would be developed as part of the programme. There was currently no allocated funding for the development and delivery of the two requested raised crossings, but officers acknowledged the concerns that had been raised in the petition. The report proposed that the requests should be added to the Requests for Traffic Management Measures Report and once funding had been allocated, scheme development could be programmed and resourced. These development processes would include the requirement for statutory public consultation, so it needed to be noted that they would not be guaranteed for delivery, even when funding became available.

At the invitation of the Chair Soren Sturup-Toft, Trustee member of the Southcote Park Estate Committee, addressed the Sub-Committee on behalf of the petitioners.

Resolved –

- (1) That the report be noted;**

- (2) That the request for waiting restrictions has been captured in the 2023B Waiting Restriction Review programme be acknowledged and that it is developed through this programme agreed;**
- (3) That the requests for raised tables at the entrances to Kenilworth Avenue and Monks Way be added to the next 'Requests for Traffic Management Measures' report update be agreed;**
- (4) That the lead petitioner be informed of the decisions of the Sub-Committee, following publication of the agreed minutes of the meeting;**
- (5) That no public inquiry be held into the proposals.**

19. PETITION UPDATE – PRIVATE HIRE VEHICLE USE OF KINGS ROAD BUS LANE

Further to Minute 16(a) of the meeting held on 14 September 2022 and Minute 28 of the meeting held on 10 November 2022 the Sub-Committee considered a report that set out the officer recommendations that related to the refined request to enable Council licenced private hire vehicles to use the Kings Road, outbound, bus lane.

The report explained that since the report that had been submitted to the November 2022 meeting of the Sub-Committee the strategy work had regrettably yet to be resourced and the petitioning private hire association had refined their request to apply only to the Kings Road (outbound) bus lane.

The report stated that the Kings Road was experiencing a relatively high number of incidents involving casualties. The majority of these incidents either involved pedestrians crossing the road or vehicles turning across other vehicles within the bus lanes. Officers were concerned that an increase in the volume of traffic in the bus lanes, particularly as this traffic would be a similar profile of vehicles to those within the general traffic lanes, would risk increasing the number of casualties. Although it was important to note that officers were not alleging that it would necessarily be as a result of any inappropriate or unsafe driving that was anticipated by the Council's licenced private hire drivers, but as a consequence of increased volumes of traffic that would be travelling at higher speeds in comparison with the general traffic lanes.

The Red Route parking restrictions that spanned from east to west Reading, including Kings Road, had primarily been implemented to improve the reliability of bus services along this corridor, particularly the Number 17 route. The alterations to the Kings Road bus lane had also been introduced with this objective, reducing the previous restriction from all private hire vehicles, alongside other permitted vehicle types, to just Reading Borough Council licenced private hire vehicles. With enforcement based on the vehicle type, and not whether the vehicle was occupied with a fare, alongside the apparent lack of a cap on either the number of licenced private hire vehicles or on the access restriction itself, opening the bus lanes to private hire vehicles could have a marked difference on traffic volumes using the lanes throughout the day.

The report explained that to pursue the requested alteration of access along the Kings Road outbound bus lane would require the following:

- Identification of funding;
- Statutory Consultation – Creation and advertising of the proposed new Traffic Regulation Order (TRO);

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- Implementation of the Decision – Consideration of the consultation feedback;
- Signing Review – Review and creation of signing specifications for the required changes along the route;
- Making the Order – Seal and advertise the made TRO;
- Implementation of the Scheme – Change the signing on street and updating exemptions on the enforcement camera software.

The report stated that, as agreed at the November 2022 meeting, it remained the intention of officers to carry out a strategic review of bus lane access across the Borough and the recommendation that the requested alteration to access along Kings Road outbound bus lane was not agreed.

At the invitation of the Chair Kamran Saddiq, Chairman of the Reading Private Hire Association, addressed the Sub-Committee in favour of private hire vehicle use of the Kings Road outbound bus lane. At the invitation of the Chair Ashif Rasheed, Chair of the Reading Taxi Association, also addressed the Sub-Committee, he spoke against private hire vehicles using the bus lane.

The Sub-Committee discussed the report and Councillor Ennis proposed, seconded by Councillor Barnett-Ward, that an informal consultation be carried out on the requested change to enable Reading Borough Council licenced private hire vehicles to use the Kings Road, outbound, bus lane. He stated that the consultation should include a number of stakeholders such as bus companies, taxi associations, cycle groups, disability access groups and the general public and a report setting out the results be submitted to the January 2024 meeting.

Resolved –

- (1) That the report be noted;**
- (2) That an informal consultation be carried out on the requested change to enable Reading Borough Council licenced private hire vehicles to use the Kings Road (outbound) bus lane;**
- (3) That a number of stakeholders be included in the informal consultation, such as, bus companies, taxi associations, cycle groups, disability access groups and the general public;**
- (4) That a report setting out the results of the informal consultation be submitted to the meeting in January 2024.**

(Councillor Ayub declared an interest in the above item on the grounds that he owned a hackney carriage. He left the room and took no part in the discussion or decision making.)

20. PROPOSALS FOR A PEDESTRIAN CROSSING – UPPER REDLANDS ROAD

The Sub-Committee considered a report that sought approval to carry out statutory consultation/notice processes necessary to progress the scheme for a pedestrian crossing on Upper Redlands Road. A scheme design of the Upper Redlands Road Pedestrian Crossing was attached to the report at Appendix 1.

The report explained that having reviewed and assessed bids from local authorities, Active Travel England had responded to authorities with an award and Reading had been awarded

funding for the delivery of the pedestrian crossing on Upper Redlands Road to the value of £75,000.

The scheme was jointly promoted by the Council, St Joseph's College and the University of Reading as there was no safe place for children, university students or local residents to cross Upper Redlands Road. This scheme would provide safer routes for children to walk to school as well as linking to the university via Elmhurst Road to the Active Travel scheme on Shinfield Road as well as the Active Travel scheme at Christchurch Green. Following discussion with Ward Councillors, officers had commissioned an independent Road Safety Audit for the scheme and, subject to the findings and necessary adjustments, which were not expected, the report proposed that officers carried out the required statutory consultation for the scheme. Any objections would be submitted to a future meeting and if there were no objections the report proposed that the scheme should be considered as approved for delivery.

Resolved –

- (1) That the report be noted;**
- (2) That Assistant Director of Legal and Democratic Services be authorised to undertake statutory consultation/notification processes for the proposed zebra crossing designs on Upper Redlands Road, in accordance with the Local Authorities Traffic Orders (Procedure) (England and Wales) Regulations 1996;**
- (3) That the Highways and Traffic Services Manager, in agreement with the Lead Councillor for Climate Strategy and Transport, be authorised to make minor alterations to the agreed proposals;**
- (4) That subject to no objections being received the scheme be considered as agreed for implementation enabling delivery planning to commence;**
- (5) That should a scheme receive objection(s) during the statutory consultation period, that these be submitted to a future meeting of the Sub-Committee for consideration and decision regarding scheme implementation.**

21. WAITING RESTRICTION REVIEW PROGRAMME

The Sub-Committee considered a report informing the Sub-Committee of objections, including petitions, that had resulted from the statutory consultation for the agreed proposals that formed the 2023A Waiting Restriction Review programme and provided the Sub-Committee with the list of new requests for potential inclusion in the 2022B programme. The following appendices were attached to the report:

Appendix 1 – Feedback that had been received to the statutory consultation for the 2023A programme, and the advertised drawings for those proposals;

Appendix 2 – Summary of the petition that had been received against the consulted 2023A programme for Ashby Court;

Appendix 3 – Summary of new requests for potential inclusion in the 2023B programme.

Resolved –

- (1) That the report be noted;**
- (2) That the following proposals made under the waiting restriction review 2023A, as set out in Appendix 1, be implemented, amended or removed from the programme as follows and the petition in Appendix 2 noted:**
 - Northumberland Avenue – Implement as advertised;**
 - Carsdale Close – Remove from the programme;**
 - Coley Avenue – Implement as advertised;**
 - Almond Drive – Implement as advertised;**
 - Odiham Avenue – Implement as advertised;**
 - Henley Road – Implement as advertised;**
 - Essex Street – Implement as advertised;**
 - Pottery Road – Implement as advertised;**
 - Helmsdale Close – Implement as advertised;**
 - Shilling Close – Implement as advertised;**
 - Ashby Court – Implement as advertised;**
 - Foxhays Road – Implement as advertised;**
 - St Agnes Way – Implement as advertised;**
- (3) That the Assistant Director of Legal and Democratic Services be authorised to seal the resultant Traffic Regulation Order;**
- (4) That respondents to the statutory consultation, and the lead petitioner, be informed of the decisions of the Sub-Committee accordingly, following publication of the agreed minutes of the meeting;**
- (5) That having considered the requests made for waiting restriction changes, set out in Appendix 3 attached to the report, the requests be investigated by officers as part of the 2023B review programme, subject to the following amendments to the programme:**
 - (i) Hemdean Road – Remove from the programme;**
 - (ii) Pottery Road – Remove from the programme;**
- (6) That that the officer recommendations, following investigation of the new requests, be shared with Ward Councillors, providing opportunity for local consultation (informal) and for their comments to be included in the next report submitted to the Sub-Committee;**
- (7) That, should funding permit, a further report be submitted to the Sub-Committee seeking agreement to conduct the Statutory Consultation on the recommended schemes for the 2023B programme;**
- (8) That no public inquiry be held into the proposals.**

22. JACKSONS CORNER – RESULTS OF STATUTORY CONSULTATION

Further to Minute 10 of the previous meeting, the Sub-Committee considered a report informing them of objections that had resulted from the statutory consultation for the proposals that had been agreed at the June 2023 meeting in respect of Jacksons Corner. The following appendices were attached to the report:

Appendix 1 – Plan to show the alterations proposed in the advertised Traffic Regulation Order;

Appendix 2 – Feedback that had been received during the statutory consultation;

Appendix 3 – Equality Impact Assessment.

The report explained that as part of the agreed planning permissions at Jacksons Corner, situated to the north-east of the junction with Kings Road and High Street, proposed alterations to the highway layout had been agreed. The alterations included widening of the narrow footway width on the northern side of Kings Road, reversal of the one-way traffic direction along Abbey Square, increased provision and relocation of bus stops and provision of on-street loading bays. The changes would necessitate alterations to existing waiting restrictions. The developer had provided the Council with funding to deliver these alterations, which it was required to complete by 31 March 2024. The delivery deadline was one of the conditions and the funding was ringfenced for delivering the scheme and, aside from potential feasibility issues in this constricted area, proposed alterations to the scheme might breach the funding conditions.

Resolved –

- (1) That the report be noted;**
- (2) That having considered the objections set out in Appendix 2, the proposals, as set out in the report, be implemented;**
- (3) That the Assistant Director of Legal and Democratic Services be authorised to seal the resultant Traffic Regulation Order,**
- (4) That the respondents to the statutory consultation be informed of the decisions of the Sub-Committee accordingly, following publication of the agreed minutes of the meeting;**
- (5) That the Highways & Traffic Services Manager, in agreement with the Lead Councillor and Ward Councillors, be authorised to make minor alterations to the proposals as may be necessary;**
- (6) That no public inquiry be held into the proposals.**

23. DIGITAL PARKING PERMITS

Further to Minute 17 of the meeting held on 14 September 2022, the Sub-Committee considered a report informing the Sub-Committee of the findings of the Digital Parking Permits Project that had been carried out between 31 October 2022 and 30 April 2023. Feedback that had been received to the statutory consultation was attached to the report at Appendix 1.

The report summarised the proposed changes that would mean permits would not be printed and posted, the scheme would no longer require a permit to be displayed in the vehicle and residents would not be asked to allow up to 14 days for these permits to be issued.

The report explained that procurement of a new Parking Services contract had led to a change in supplier, the new service would start on 1 November 2023 with Unity 5 providing back office Penalty Charge Notice (PCN) and Permit software system. This had provided for improvements to be made, which allowed for some of the issues that had been identified at pilot stage to be resolved.

All permits would need to be migrated to the new system regardless of whether they were digital or not and it should be noted that even if the permits were still printed, they would still be linked to the Civil Enforcement Officers handheld computer terminal, except visitor permit scratch cards. Visitor permits would not be linked to the handheld terminal and if there were any errors with the completion of the permit the visitor would be liable to receive a PCN, as they did currently. They would also receive a PCN if the permit was obscured/fell down. The Council was unlikely to cancel PCNs that had been incorrectly completed or not displayed correctly. A digital visitor permit scheme would remove the likelihood of a PCN being issued because the vehicle and dates were completed online and there was no issue with the permits falling down.

The report included a table that showed the permits that had been issued between 31 October 2022 and 30 April 2023 and the total on issue as of 5 July 2023. The report also included an analysis of the consultation feedback and a summary of the benefits of the new scheme.

Finally, the report proposed that digital permits should be rolled out for all permit types, except for visitor permits, to continue to have offline (physical) permits for residents who still needed this requirement (specifically for Resident, Carers and Visitor Permits), to continue the trial in Permit Zone 02R for digital permits and to update the permit scheme rules and definitions with these changes.

Resolved –

- (1) That the proposal that Digital Permits were introduced Boroughwide for all permits except Visitor Permits, which would remain as physical permits, be agreed;**
- (2) That offline (physical) permits be retained for vulnerable residents (residents & carer permits);**
- (3) That residents in the permit zone 02R which was part of the original trial continue to trial digital visitor permits under the new permit software;**
- (4) That a statutory consultation for the implementation of a full rollout of digital permits Boroughwide to include all permit types be carried out in November 2023 and the Assistant Director of Legal and Democratic Services be granted authority to make the Traffic Regulation Order;**
- (5) That the Traffic Regulation Order be made permanent;**

- (6) That respondents to the statutory consultation be informed of the decisions of the Sub-Committee accordingly following publication of the agreed minutes of the meeting;**
- (7) That no public inquiry be held into the proposal.**

24. EXCLUSION OF PRESS AND PUBLIC

Resolved –

That, pursuant to Section 100A of the Local Government Act 1972 (as amended) members of the press and public be excluded during consideration of item 25 below, as it was likely that there would be disclosure of exempt information as defined in Paragraphs 1 and 2 of Part 1 of Schedule 12A of that Act.

25. APPLICATIONS FOR DISCRETIONARY PARKING PERMITS

The Sub-Committee considered a report giving details of the background to the decisions to refuse applications for Discretionary Parking Permits from 19 applicants, who had subsequently appealed against these decisions.

Resolved –

- (1) That, with regard to applications 1 and 11, a first discretionary permit be issued, personal to the applicants;**
- (2) That, with regard to applications 15, 16 and 19, a first discretionary permit be issued, personal to the applicant, subject to the applicant providing the required proofs and documentation;**
- (3) That, with regard to application 17, a first discretionary permit be issued for one year, personal to the applicant, subject to the applicant providing the appropriate proofs and documentation;**
- (4) That, with regard to applications 3 and 13, discretionary visitor permit books be issued, personal to the applicants, subject to the standard scheme limits for the number of books that could be issued each year and charged at the standard rate;**
- (5) That the Executive Director for Economic Growth and Neighbourhood Services' decision to refuse applications 2, 4, 5, 6, 7, 8, 9, 10, 12, 14 and 18 be upheld.**

(Exempt information as defined in Paragraphs 1 and 2).

(The meeting started at 6.30 pm and finished at 8.33 pm).

Traffic Management Sub-Committee

13 November 2023



Reading
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Title	Petition – Hemdean House School
Purpose of the report	To note the report for information
Report status	Public report
Report author	James Penman, Network Services Manager, Network Services
Lead councillor	John Ennis
Corporate priority	Healthy Environment
Recommendations	<p>The Committee is asked to:</p> <ol style="list-style-type: none"> 1. Note the content of this report. 2. Note that officers will consider the content of the petition and provide a petition response report to a future meeting.

1. Executive summary

- 1.1. To report to the Sub-Committee the receipt of a petition from Hemdean House School.
- 1.2. While at the time of writing the detailed content of the petition is not yet known by officers, it is expected to request measures to reduce safety risks outside the school on Hemdean Road. There has been good engagement between the school, Ward Councillors, and officers around potential measures.
- 1.3. Officers will consider the content of the petition and provide a petition response report to a future meeting. Resultant agreed measures will require funding, so it is likely that the petition response report will recommend a new/amendment to an existing entry onto the 'Requests for Traffic Management Measures' report.

2. Policy context

- 2.1. The recommendations of this report will not directly lead to the introduction of changes. However, the implementation of risk reduction measures, such as traffic calming, would be expected to align with the following theme in the Council's Corporate Plan for the years 2022/25:
 - Healthy Environment

3. The proposal

Current Position

- 3.1. Representatives from Hemdean House School have been in contact with Ward Councillors and officers, following their review of Hemdean Road and considering changes that could reduce risks.

These proposals primarily included speed reduction measures, such as 20mph and traffic calming, signage as well as cycling improvements. The school has been provided with high-level feedback to inform their further review of desirable changes and notified the Council of their intention to bring a petition to this Sub-Committee meeting.

- 3.2. At the time of writing, officers are not aware of the full content for the petition.
- 3.3. For context, the section of Hemdean Road in the vicinity of the school is open to two-way traffic including a scheduled bus route. There is a slight bend in the road as it passes the school and there is on-street parking on both sides of the road to the north and south of the school. Parking is restricted immediately outside the school by the provision of 'School Keep Clear' markings.

Within the latest 3-year period of Police-supplied casualty data (period up to 1st June 2023), there are no recorded incidents on Hemdean Road between its junctions with Queen Street and Hemdean Hill. Officers therefore consider requested measures to be in the context of risk reduction, rather than casualty reduction.

- 3.4. The regular 'Requests for Traffic Management Measures' report contains an entry for a desired area 20mph zone in lower Caversham. This request is line 69, Appendix 3 of the report to this Sub-Committee, which is also available on our website [here](#). This line refers to an earlier report that proposed a concept area including the section of Hemdean Road outside the school and this report (and concept area plan) are available on our website [here](#).

This 'Requests for Traffic Management Measures' report typically comes to this Sub-Committee twice-annually and captures requests for traffic management schemes that do not currently have identified funding. Schemes originating from this list have attracted funding nominations, including those from Local 15% Community Infrastructure Levy and successful government funding bids, such as the Active Travel Tranche 4.

The lower Caversham 20mph zone concept was developed at a time when significant developer contributions were expected. Unfortunately, these did not materialise, however, this is still a desirable scheme for development and delivery. While the scale of the concept area is such that it would require significant funding, smaller funding nominations could contribute to a phased delivery on an area priority basis.

Options proposed

- 3.5. It is proposed that officers consider the content of the petition and provide a petition response report to a future meeting of this Sub-Committee. This report will contain recommendations for the next steps.
- 3.6. It is likely that the recommendation of the petition response report will be a new entry, or amendment to an existing entry, on the aforementioned 'Requests for Traffic Management Measures' report. Officers will make recommendations about what measures should be added to this report, following consideration of the petition contents and professional recommendations in response.

Other options considered

- 3.7. None at this time.

4. Contribution to strategic aims

- 4.1. The Council's new Corporate Plan has established three themes for the years 2022/25. These themes are:
- Healthy Environment
 - Thriving Communities
 - Inclusive Economy

- 4.2. These themes are underpinned by “Our Foundations” explaining the ways we work at the Council:
- People first
 - Digital transformation
 - Building self-reliance
 - Getting the best value
 - Collaborating with others
- 4.3. Full details of the Council’s Corporate Plan and the projects which will deliver these priorities are published on the [Council’s website](#). These priorities and the Corporate Plan demonstrate how the Council meets its legal obligation to be efficient, effective and economical.
- 4.4. The recommendation of this report does not directly deliver changes. Requests for new traffic management measures would need to be considered alongside the Borough Council’s Strategic Aims, the Local Transport Plan (LTP), and Local Cycling, Walking and Infrastructure Plan (LCWIP).

5. Environmental and climate implications

- 5.1. The Council declared a Climate Emergency at its meeting on 26 February 2019 (Minute 48 refers).
- 5.2. The recommendation of this report does not directly deliver changes, so a Climate Impact Assessment has not been considered necessary at this time.

6. Community engagement

- 6.1. The lead petitioner will be informed of the decision of the Sub-Committee regarding the request that they have made, following publication of the meeting minutes.
- 6.2. Meeting reports and minutes are published on the Council’s website and Traffic Management Sub-Committee is a public meeting that can be attended. Recordings of the meetings are also available via the Council’s website (www.reading.gov.uk).
- 6.3. Ward Councillors and officers have been engaging with representatives of Hemdean House School leading up to this Sub-Committee meeting.

7. Equality impact assessment

- 7.1. Under the Equality Act 2010, Section 149, a public authority must, in the exercise of its functions, have due regard to the need to—
- eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under this Act;
 - advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it;
 - foster good relations between persons who share a relevant protected characteristic and persons who do not share it.
- 7.2. It is not considered that an Equality Impact Assessment is relevant at this time as the report recommendation does not directly lead to any physical change. Assessment will be considered once funding for development and delivery of a scheme is identified.

8. Other relevant considerations

- 8.1. None expected from the recommendations and decisions for this report.

9. Legal implications

- 9.1. There are no foreseen legal implications relating to the recommendation of this report.

10. Financial implications

10.1. None arising from the recommendation of this report.

11. Timetable for implementation

11.1. Not applicable.

12. Background papers

12.1. There are none.

Appendices –

There are none.

Traffic Management Sub-Committee

13 November 2023



Reading
Borough Council
Working better with you

Title	Petition – Holmes Road Traffic Plug
Purpose of the report	To make a decision
Report status	Public report
Report author	James Penman, Network Services Manager, Network Services
Lead councillor	John Ennis
Corporate priority	Healthy Environment
Recommendations	<p>The Committee is asked to:</p> <ol style="list-style-type: none"> 1. Note the content of this report. 2. Agree to the existing entry on the 'Requests for Traffic Management Measures' report being updated to reflect the receipt of this petition. 3. Agree to the lead petitioner being informed of the decisions of the Sub-Committee, following publication of the agreed minutes of the meeting. 4. Agree that no public inquiry be held into the proposals.

1. Executive summary

- 1.1. To report to the Sub-Committee the receipt of a petition requesting the Council to install a one-way traffic plug on Holmes Road, at its junction with Wokingham Road, to tackle reported issues of speeding and through- traffic.
- 1.2. The report notes that there is an existing entry on the regularly reported 'Requests for Traffic Management Measures' and recommends that this entry is updated to reflect the receipt of this petition. This report entry is recorded for future funding allocation and the next report update is expected at the March 2024 meeting of this Sub-Committee.

2. Policy context

- 2.1. The recommendations of this report will not directly lead to changes being introduced. However, the implementation of such a traffic calming scheme would be expected to align with the following theme in the Council's Corporate Plan for the years 2022/25:
 - Healthy Environment

3. The proposal

Current Position

- 3.1. On 2 November 2023, a petition was submitted to the Council containing 27 signatories.

The wording of the petition stated the following:

The residents of Holmes Road, who have signed below are petitioning for the installation of a one-way plug to prevent speeding traffic entering Holmes Rd. from the Wokingham Rd. The current volumes and speed of traffic in Holmes Rd is putting lives at risk. We believe that a plug would go some way to reducing the risk of serious accidents in Holmes Rd.

Appendix 1 contains the following:

- The front page of the petition, with personal data redacted;
- Supplementary document that was provided with the petition; and
- Photographs and illustration that were provided with the petition.

The Sub-Committee is asked to note that photographs of a recent casualty incident were also included with the petition, which have not been included as part of this public report.

- 3.2. The 'Requests for Traffic Management Measures' report that comes to this Sub-Committee twice-annually contains an entry for the one-way plug requested in this petition. The latest update for this report is at this Sub-Committee meeting, where the entry can be found as item 84 on Appendix 3 (report available [here](#)).

This report captures requests for traffic management schemes that do not currently have identified funding. Schemes originating from this list have attracted funding nominations, including those from Local 15% Community Infrastructure Levy and successful government funding bids, such as the Active Travel Tranche 4.

Options proposed

- 3.3. Officers acknowledge the concerns that have been raised in this petition, however, there is currently no allocated funding for the development nor delivery of this requested feature.
- 3.4. It is recommended that the report entry referred in Item 3.2 be updated to reflect the receipt of this petition, with expectation that scheme development can be programmed once funding has been identified.

Other options considered

- 3.5. None at this time.

4. Contribution to strategic aims

- 4.1. The Council's new Corporate Plan has established three themes for the years 2022/25. These themes are:
- Healthy Environment
 - Thriving Communities
 - Inclusive Economy
- 4.2. These themes are underpinned by "Our Foundations" explaining the ways we work at the Council:
- People first
 - Digital transformation
 - Building self-reliance
 - Getting the best value
 - Collaborating with others
- 4.3. Full details of the Council's Corporate Plan and the projects which will deliver these priorities are published on the [Council's website](#). These priorities and the Corporate Plan demonstrate how the Council meets its legal obligation to be efficient, effective and economical.

- 4.4. The recommendation of this report does not directly deliver changes. Requests for new traffic management measures would need to be considered alongside the Borough Council's Strategic Aims, the Local Transport Plan (LTP), and Local Cycling, Walking and Infrastructure Plan (LCWIP).

5. Environmental and climate implications

- 5.1. The Council declared a Climate Emergency at its meeting on 26 February 2019 (Minute 48 refers).
- 5.2. The recommendation of this report does not directly deliver changes, so a Climate Impact Assessment has not been considered necessary at this time.

6. Community engagement

- 6.1. The lead petitioner will be informed of the decision of the Sub-Committee regarding the request that they have made, following publication of the meeting minutes.
- 6.2. Meeting reports and minutes are published on the Council's website and Traffic Management Sub-Committee is a public meeting that can be attended. Recordings of the meetings are also available via the Council's website (www.reading.gov.uk).

7. Equality impact assessment

- 7.1. Under the Equality Act 2010, Section 149, a public authority must, in the exercise of its functions, have due regard to the need to—
- eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under this Act;
 - advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it;
 - foster good relations between persons who share a relevant protected characteristic and persons who do not share it.
- 7.2. It is not considered that an Equality Impact Assessment is relevant at this time as the report recommendation does not directly lead to any physical change. Assessment will be considered once funding for development and delivery of a scheme is identified.

8. Other relevant considerations

- 8.1. None expected from the recommendations and decisions for this report.

9. Legal implications

- 9.1. There are no foreseen legal implications relating to the recommendation of this report.

10. Financial implications

- 10.1. None arising from the recommendation of this report.

11. Timetable for implementation

- 11.1. Not applicable.

12. Background papers

- 12.1. There are none.

Appendices –

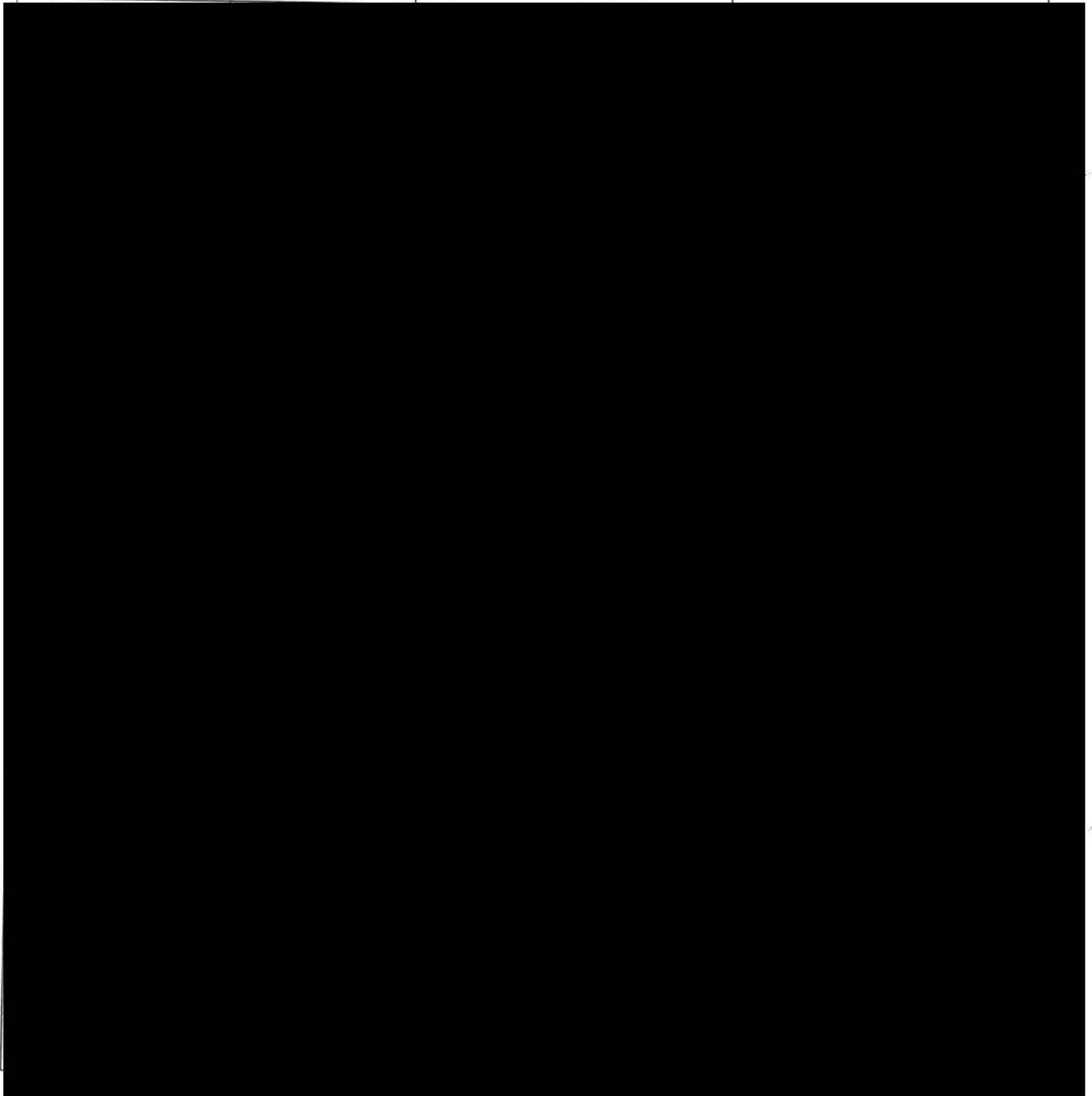
1. Redacted petition sheet and supplementary documents included with the petition submission.

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HOLMES ROAD PETITION FOR A ONE-WAY PLUG

The residents of Holmes Road, who have signed below are petitioning for the installation of a one-way plug to prevent speeding traffic entering Holmes Rd. from the Wokingham Rd. The current volume and speed of traffic in Holmes Rd is putting lives at risk. We believe that a plug would go some way to reducing the risk of serious accidents in Holmes Rd.

NAME	HOUSE number	e-mail	SIGNATURE
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REASONS TO INSTALL A PLUG

THE SPEED AND VOLUME OF TRAFFIC

- This has definitely increased over the last two years. The proposed ECMWF development at Earley Gate will add to the volume of traffic with an expected 300 staff and 1,900 visitors each year.
- Speeding traffic has easy access to Holmes Rd. due to the wide openings at each end of the street allowing the vehicles to maintain speed around the corners.
- The wide ends also encourage large trucks, delivery vans and lorries to turn easily into Holmes Rd.
- Most vehicles are using the street as a cut through to avoid traffic signals on the Wokingham Rd and congestion at Wilderness/Whiteknights Rd round-a-bout.

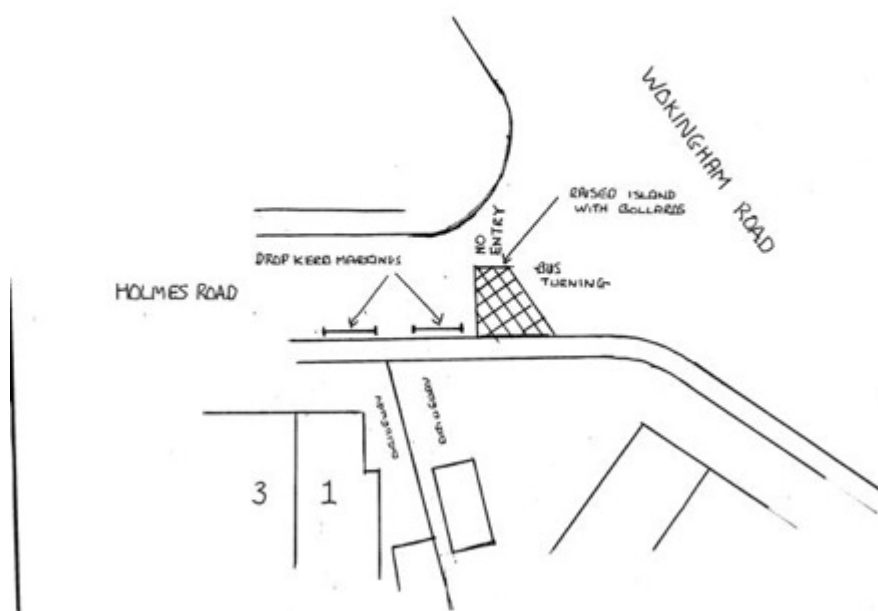
THE PROBLEMS CAUSED

- Difficulties in crossing the road for all but especially school children on their way to school, mothers with buggies and disabled residents.
- Confusing manoeuvres of buses, cars and cyclists at the Wokingham Rd end has caused at least one serious RTA
- Residents who have driveways put themselves in danger as they try to pull out into the road while avoiding being hit by speeding traffic or boxed in by traffic not giving way.
- Vehicles have passing problems in the street, which leads to aggressive behaviour and damage to parked vehicles.

SUGGESTIONS FOR ALLEVIATING THESE DANGERS

- Construct a one-way plug (similar to the one on Talfourd Avenue) at the Wokingham Rd.end (as seen on the diagram attached)
- A narrowing of the wide access into and out of Whiteknights Rd.





Traffic Management Sub-Committee

13 November 2023



Reading
Borough Council
Working better with you

Title	Parking Restrictions at Former Retail Park Exit Chatham Street
Purpose of the report	To make a decision
Report status	Public report
Report author	Darren Cook
Lead councillor	John Ennis
Corporate priority	Not applicable, but still requires a decision
Recommendations	<p>The Committee is asked to:</p> <ol style="list-style-type: none"> 1. That the Sub-Committee notes the report. 2. That the Assistant Director of Legal and Democratic Services be authorised to undertake a statutory consultation in accordance with the Local Authorities Traffic Orders (Procedure) (England and Wales) Regulations 1996, for the proposals contained within in Appendix 1. 3. That subject to no objections being received, the Assistant Director of Legal and Democratic Services be authorised to make the Traffic Regulation Order for the proposed scheme. 4. That any objection(s) received following the statutory advertisement be reported to a future meeting of the Sub-Committee. 5. That the Head of Transport (or appropriate Officer) in consultation with the appropriate Lead Councillor, be authorised to make minor changes to the proposals. 6. That no public inquiry be held into the proposals.

1. Executive summary

- 1.1. To report to the Sub-Committee traffic management measures associated with the development at the Former Wickes site on Weldale Street/Chatham Street.
- 1.2. This report seeks approval from the Sub-Committee to carry out a Statutory Consultation on the introduction of loading restrictions within a vehicular exit onto Chatham Street related to the retail park that was previously occupied by Wickes and Iceland.
- 1.3. The proposal is illustrated on Drawing 04243 B 1200 Revision A9 which can be found at Appendix 1 with an inset of that drawing showing the details clearer at Appendix 2.

2. Policy context

- 2.1. The Council's new Corporate Plan has established three themes for the years 2022/25. These themes are:
 - Healthy Environment
 - Thriving Communities

- Inclusive Economy
- 2.2. These themes are underpinned by “Our Foundations” explaining the ways we work at the Council:
- People first
 - Digital transformation
 - Building self-reliance
 - Getting the best value
 - Collaborating with others
- 2.3. Full details of the Council's Corporate Plan and the projects which will deliver these priorities are published on the [Council's website](#). These priorities and the Corporate Plan demonstrate how the Council meets its legal obligation to be efficient, effective and economical.

3. The proposal

- 3.1. Planning permission was granted in March 2018 for the redevelopment of the former Wickes/Iceland site bounded by Weldale Street to the north and Chatham Street to the south. The permission included the provision of 427 residential units (Class C3) and 1 flexible ground floor commercial unit, planning reference 170326. The first phase of development has been completed with the second phase having commenced.
- 3.2. As a result of the development a S278 Highway Works Agreement was necessary which among other things was to close off the historic exit from the retail park on Chatham Street albeit that some egress is to be retained. The design ensures that vehicles can exit but the area is secured by way of bollards making the ramp mainly for the use of pedestrians.
- 3.3. The proposal subject to this report consists of providing a double yellow line no loading or unloading at any time restriction across the former exit to tie into existing restrictions either side of the former exit with the exiting no waiting restriction to the east revised to also include no loading or unloading. The inclusion of the waiting restrictions was deemed necessary to ensure that indiscriminate parking or loading does not occur along the Chatham Street frontage of the development causing obstructions to the flow of traffic along Chatham Street and the intervisibility between pedestrians and drivers at the zebra crossing located at the Chatham Street/Friar Street/IDR roundabout junction.
- 3.4. The loading restrictions are therefore essential to dissuade drivers from parking vehicles within the recessed exit and close to the existing zebra crossing.
- 3.5. It is therefore requested that a statutory consultation be permitted in order to facilitate these waiting restrictions, the extent of which are illustrated on Drawing 04243 B 1200 Revision A9 and can be found at Appendix 1 and 2.
- 3.6. It should be noted that a specific drawing will be provided for the consultation process but this has not been possible prior to the committee.

4. Contribution to strategic aims

- 4.1. This proposal contributes to the Council's Corporate Plan Themes as set out below:

Healthy Environment

Waiting restrictions can assist in preventing obstructive, hazardous or other nuisance parking. In some situations, inconsiderate parking can compromise safety or result in difficulties for residents and businesses. Many parking issues can create delays or accessibility obstructions for users of the network such as pedestrians, cyclists, domestic vehicles, delivery vehicles, emergency services and public transport.

The proposals promoted through the proposed alterations can help to reduce some of these parking issues. They can lead to more efficient traffic flow, clearer footways, improvements to perceived Highway safety and greater containment. These can lead to lower vehicle emissions and the removal of barriers toward the greater use of sustainable

and healthy transport modes. The proposals will contribute to the Council's goal of making the town carbon neutral by 2030.

5. Environmental and climate implications

- 5.1. The Council declared a Climate Emergency at its meeting on 26 February 2019 (Minute 48 refers).
- 5.2. A climate impact assessment has been conducted for the recommendations of this report.
- 5.3. There has been some minor negative impact for investigation and design, through travel and energy usage. Travel impacts have been mitigated by Officers travelling to the site through walking and cycling. Advertised notices need to be weatherproof and are, therefore, not typically recyclable. The implementation of schemes currently requires burning of fossil fuels for the specialist machinery and some road marking application/removal techniques.
- 5.4. The making of this permanent TRO will require (by regulation) advertisement of the legal Notice in the local printed newspaper, which will have a negligible, one-off impact in terms of likely additional printing and paper usage.
- 5.5. However, it is expected that these relatively minor negative impacts over a short period of time will be more than overcome by the benefits of scheme implementation. The proposals cover perceived local safety, accessibility and traffic flow issues that, once resolved, should improve traffic flow (lower emissions) within the vicinity of the development.

6. Community engagement

- 6.1. Any Statutory consultation will be carried out in accordance with the Local Authorities Traffic Orders (Procedure) (England and Wales) Regulations 1996, advertised on street, in the local printed newspapers and on the Council's website (the 'Consultation Hub'). Notices will be advertised in the local printed newspaper and will be erected, typically on lamp columns, as close as possible to affected area.

7. Equality impact assessment

- 7.1. Under the Equality Act 2010, Section 149, a public authority must, in the exercise of its functions, have due regard to the need to—
 - eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under this Act;
 - advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it;
 - foster good relations between persons who share a relevant protected characteristic and persons who do not share it.
- 7.2. It is not considered that an Equality Impact Assessment is relevant as the proposals are not deemed to be discriminatory to persons with protected characteristics. A statutory consultation will be conducted, providing an opportunity for objections/support/concerns to be considered prior to a decision being made on whether to implement the proposals. Waiting Restrictions can have a positive impact whereby the roads are made safer for all users as locally problematic parking issues are reduced.

8. Other relevant considerations

- 8.1. Not Applicable.

9. Legal implications

- 9.1. New, or changes to existing, Traffic Regulation Orders require advertisement and consultation, under the Road Traffic Regulation Act 1984 and in accordance with the Local Authorities Traffic Orders (Procedure) (England and Wales) Regulations 1996. The resultant Traffic Regulation Order will be sealed in accordance with the same regulations.

10. Financial implications

- 10.1. Funding for the statutory consultation comes from S278 Highways Agreement which was secured to facilitate the waiting restrictions described above. The implementation of the waiting restrictions will be undertaken by the developer by way of the Section 278 Agreement, which is in place to secure alterations to the existing Highway.

11. Timetable for implementation

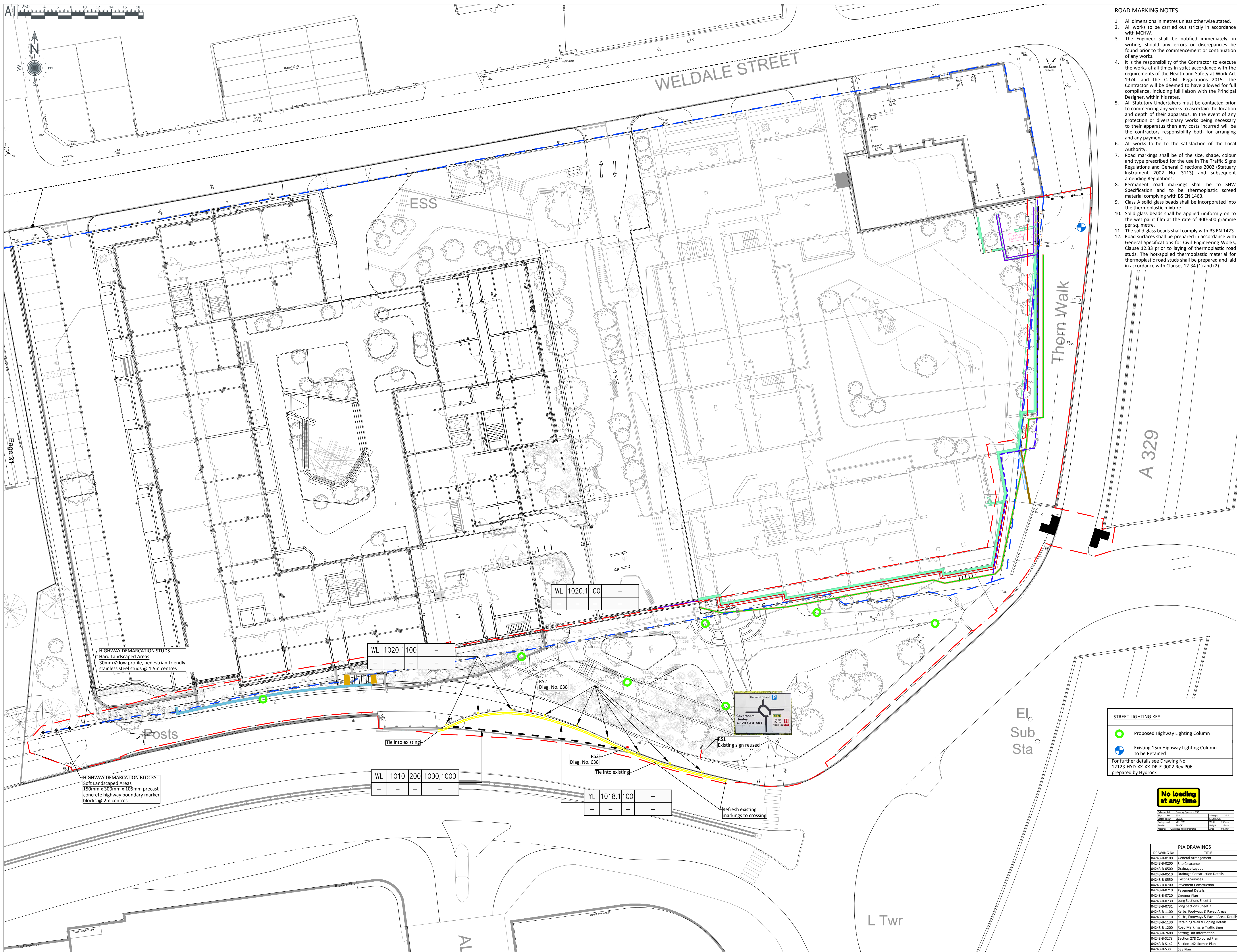
- 11.1. The new access has been constructed by the developer and the new lining will be installed by the developers' contractors as soon as possible post consultation on the proposed waiting restrictions.

12. Background papers

- 12.1. There are none.

Appendices

1. **Drawing 04243 B 1200 Revision A9**
2. **Inset of Drawing 04243 B 1200 Revision A9**



ROAD MARKING NOTES

2. All dimensions in metres unless otherwise stated.
2. All works to be carried out strictly in accordance with MCHW.
3. The Contractor shall be notified immediately, in writing, should any errors or discrepancies be found prior to the commencement or continuation of any works.
4. It is the responsibility of the Contractor to execute the works within the time in strict accordance with the requirements of the Health and Safety Act at Work Act 1974, and the C.D.M. Regulations 2015. The Contractor will be deemed to have allowed for full compliance, including full liaison with the Principal Designer and his staff.
5. All Statutory Undertakers must be contacted prior to commencing any works to ascertain the location and depth of their apparatus. In the event of any damage to their apparatus being necessary, the Contractor to their apparatus then any costs incurred will be the contractors responsibility both for arranging and any payment.
6. All works to be to the satisfaction of the Local Authority.
7. Road markings shall be of the size, shape, colour and type prescribed for the use in the Traffic Signs Regulations and General Directions 2002 (Statutory Instrument 2002 No. 3113) and subsequent amendments.
8. Permanent road markings shall be to SHW Specification and to be thermoplastic spread material complying with BS EN 1463.
9. Glass road studs shall be incorporated into the thermoplastic mixture.
10. Solid glass beads shall be applied uniformly on to the wet paint film at the rate of 400-500 grammes per sq. metre.
11. Thermoplastic beads shall comply with BS EN 1423.
12. Road surfaces shall be prepared in accordance with General Specifications for Civil Engineering Works, Clause 12.3.3 prior to laying of thermoplastic road surface. Thermoplastic road surface shall be laid in accordance with Clause 12.3.4 (1) and (2).

NOTES

These drawings have been produced with reference to the DRA Regulations 2015. Please note that these are pre-construction phase drawings and should be subject to further design risk management as required in accordance with Regulation 9.

The residual risks identified on the DRA / drawings are based on the information available at the time of design. Any services information indicated on PJA drawings is obtained from third parties, and is given for guidance purposes only. The exact location of any services will need to be verified by the client / contractor before commencing works on site.








GENERAL NOTES:

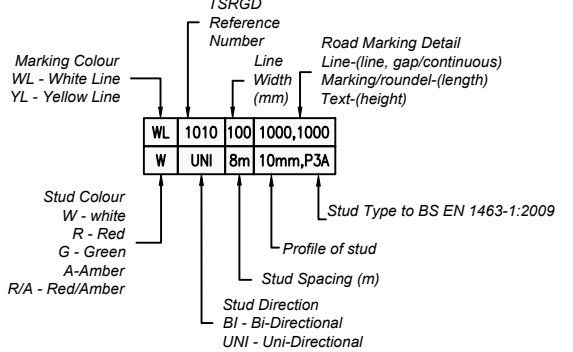
- a. This drawing is to be read in conjunction with all other relevant Engineering and Architect's details.
- b. If any errors are discovered immediately, in writing, should any errors or discrepancies be found prior to the commencement or continuation of any works.
- c. All work is to be carried out in accordance with current British Standards, Building Regulations and NIBIC Standards.
- d. It is the responsibility of the Contractor to execute the works at all times in strict accordance with the requirements of the Health and Safety at Work Act 1974, and the C.O.M. Regulations 2015. The Contractor shall be deemed to have allowed for full compliance within his rates.
- e. Any existing details which are shown on this drawing are for information only. Any variations or changes to the existing details or drawings are to be recorded and reported to the engineer immediately.
- f. Before work commences contractor should consult the engineer for the full report regarding any contamination issues. All necessary Health and Safety measures to be taken.

SECTION 278 WORKS:

2. All works to be carried out strictly in accordance with the Local Authority Highways specification for street works.
3. All contractors engaged in the works must be registered with the relevant companies, must be contacted prior to commencing any works to ascertain the correct depth of the existing surface and the extent of any protection or diversionary works being necessary to their apparatus then any costs incurred will be charged to the contractor.
4. The Contractor shall provide, erect and maintain such traffic management and safety measures as may be necessary to ensure the safety of the public and any measures may be necessitated by the construction of the works in accordance with the recommendations contained in Chapter 8 of the Manual of Streets and Roads.
5. The developer and contractor shall confirm they are working in accordance with the relevant British Standards, the British Standards and Colour Coding of Underground Utilities' Apparatus and Volume 2 Guidelines on the Positioning of Underground Utilities.
6. Retention of the existing kerbline will be subject to inspection and approval by the Local Authority Highways Inspector.
7. Re-use of the existing kerb race will be subject to inspection and approval by the Local Authority Highways Inspector.
8. The Contractor shall ensure that the proposed works do not overlie or cross any subterranean layer that is disturbed.
9. The Contractor shall ensure that any calculations may be required to determine sub-base thickness.
10. Hinged gully grate and frame to BS EN 124, Class D400, not less than 600mm x 600mm, with a minimum depth of 100mm. Minimum waterway area 900cm squared. Frame depth 100mm. Backfill to BS EN 124.
11. Any existing gullies that are damaged during construction will be replaced with new ones, as agreed with a representative of the Local Authority.
12. Hand laying of bituminous materials is not permitted on the road surface.
13. Tarmac road construction to comply with BS EN 13180 for

S278 GENERAL KEY

- | | |
|---|---|
|  | Existing Highway Boundary - Retained |
|  | Existing Highway Boundary - to be Amended |
|  | Proposed Highway Boundary |
|  | Section 278 Works Boundary |
|  | Proposed Bollards
(R denotes removable) |
|  | Highway Demarcation Blocks in Soft
Landscape Areas |
|  | Highway Demarcation Blocks in Hard
Landscape Areas |



09	21.04.22	Street lighting layout updated to reflect Hydrok Drawing No 1213-H124-XXX-XX-DR-9502-306	GL
10	21.04.22	Street lighting layout updated to reflect Hydrok Drawing No 1213-H124-XXX-XX-DR-9502-306	GL
11	07.02.22	5278 Works Boundary amended	NS
12	01.09.21	5278 works boundary amended to include new tactile paving to top of A329 slip road	NS
13	26.07.21	Highway boundary amended. Street lighting updated to reflect Hydrok Drawing No 1213-H124-XXX-XX-DR-9502-306	NS
14	15.06.21	Street lighting layout updated to reflect Hydrok Drawing No 1213-H124-XXX-XX-DR-9502-306	NS
15	27.05.21	Road markings amended to include yellow line crossing RS2 details updated Highway & Road 5278 works boundary amended.	NS
16	09.03.21	Additional road signs added to site detail added. Highway boundary amended.	NS
17	14.10.20	Additional road signs added and blocks added. Double yellow lines extended. Additional PFA drawings added to table.	NS
18	01.04.20	Drawing updated to include Diag. No. 10201 and Diag. No. 638. Ten new drawings added to yellow lines table.	NS

STREET LIGHTING KEY

- Proposed Highway Lighting Column
- Existing 15m Highway Lighting Column

For further details see Drawing No
12123-HYD-XX-XX-DR-E-9002 Rev P
prepared by Hydrock

**No loading
at any time**

PIA DRAWINGS	
DRAWING NO	TITLE
04243-0-0100	General Arrangement
04243-02-0000	Site Clearance
04243-03-0000	Drainage Layout
04243-03-0510	Drainage Construction Details
04243-03-0550	Existing Services
04243-03-0700	Pavement Construction
04243-03-0710	Pavement Details
04243-03-0720	Contour Plan
04243-03-0730	Long Sections Sheet 1
04243-03-0731	Long Sections Sheet 2
04243-03-1100	Kerbs, Footways & Paved Areas
04243-03-1110	Kerbs, Footways & Paved Areas Details
04243-03-1130	Retaining Wall & Coping Details
04243-03-1200	Road Markings & Traffic Signs
04243-03-2600	Setting Out Information
04243-03-5278	Section 278 Coloured Plan
04243-03-5142	Section 142 Licence Plan
04243-03-538	S38 Plan

PJA  Seven House - High Street
Longbridge - Birmingham
B31 2UQ - Tel: 0121 475 0234

Birmingham - Bristol
Exeter - London - Reading
pja.co.uk

Ropemaker Properties Ltd.

PROJECT

Foundry Quarter Reading

DRAWING TITLE

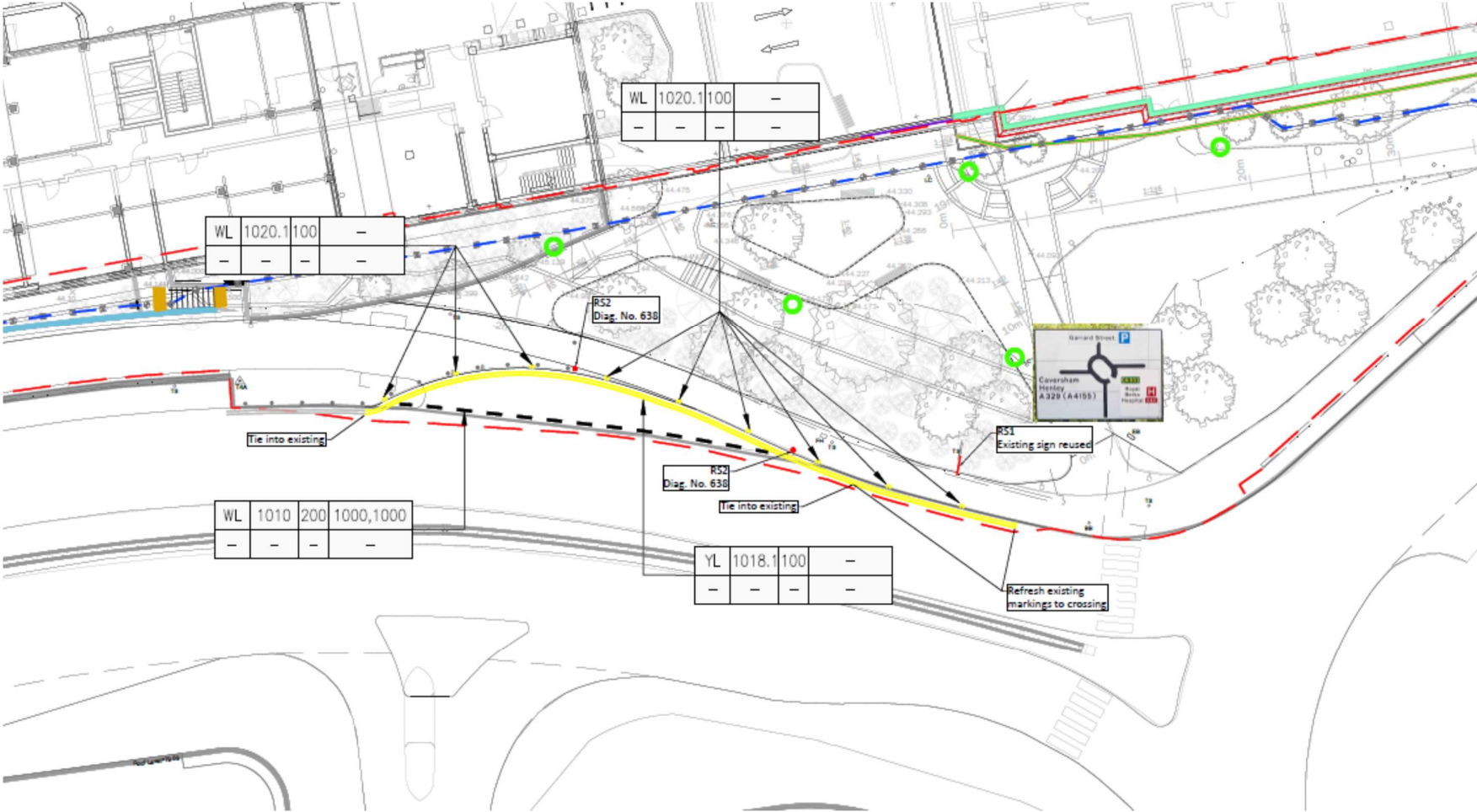
Chatham Street
Section 278 Works
Road Markings & Traffic Signs

DRAWING ISSUE STATUS			
CONSTRUCTION			
PROJECT No.	SUB-CODE	DRAWING NO.	REVISION
04243	- B	- 1200	- A9
Revision Letter : P - Prelim / A - Approval / T - Tender / C - Construction			
Drawing Reference			

SCALE	DRAWN	REVIEWED	DATE
1:250	NS	RMB	12.11.19

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Appendix 2 – Inset of Drawing 04243 B 1200 Revision A9



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Traffic Management Sub-Committee

13 November 2023



Reading
Borough Council
Working better with you

Title	Requests for Traffic Management Measures
Purpose of the report	To make a decision
Report status	Public report
Report author	James Penman – Network Services Manager
Lead Councillor	John Ennis
Corporate priority	Healthy Environment
Recommendations	<p>The Committee is asked to:</p> <ol style="list-style-type: none"> 1. Note the content of this report. 2. That the Sub-Committee considers the officer recommendation for each request in Appendix 1 and takes a decision on whether to remove or retain these entries on the primary list of requests (Appendix 3). 3. That the Sub-Committee considers the officer recommendation for amendments to each request in Appendix 2 and takes a decision on whether to remove or retain these amended entries on the primary list of requests (Appendix 3). 4. That the Sub-Committee may wish to consider whether any previously reported items in Appendix 3 can be agreed for removal.

1. Executive Summary

- 1.1. This report informs the Sub-Committee of requests for traffic management measures that have been raised with officers. These are measures that have either been previously reported, or those that would not typically be addressed in other programmes, where funding is yet to be identified.
- 1.2. The Sub-Committee is asked to consider the officer recommended action for each item in Appendix 1 and Appendix 2. Appendix 3 is for information only – this is the principal list of requests.
- 1.3. Appendix 1 – Provides the list of requests that are new to this update report with initial officer comments and recommendations.

Appendix 2 – Provides the list of requests that have been previously-reported, where significant amendments are proposed, with officer comments and recommendations.

Appendix 3 – For information. Provides the principal list of requests, as updated following the previous report to the Sub-Committee in March 2023. It also contains the prioritised list of cycling and walking measures from the LCWIP.

2. Policy Context

- 2.1. The proposals align with the following theme in the Council's Corporate Plan for the years 2022/25.
 - Healthy Environment

- 2.2. Requests for new measures would need to be considered alongside the Borough Council's Traffic Management Policies and Standards and Strategic Aims, the Local Transport Plan (LTP), and Local Cycling, Walking and Infrastructure Plan (LCWIP). Many of the proposals will complement the Council's Climate Emergency Strategy and Health and Wellbeing Strategy by removing barriers to the greater use of sustainable, healthy transport options.

3. The Proposal

- 3.1. The Council receives many requests for new traffic management measures across the borough and has several programmes in which they may be addressed. Such programmes include the Waiting Restriction Review, Resident Permit Parking and Road Safety. However, funding to address desirable general traffic management measures is harder to secure.
- 3.2. This report does not necessarily affect major strategic transport and cycling schemes that are funded as a part of any major scheme project award from central Government and/or the Local Enterprise Partnership. It does, however, include requests that are received by several Council departments and includes requests made by the Cycle Forum.
- 3.3. Appendix 3 provides the primary list of requested schemes and requests for measures, which is currently held by officers. It is likely that the primary sources of funding for these schemes will be local CIL contributions and other third-party contributions. If funding has been allocated to a scheme, the entry will be removed from this list and added to Appendix 2, seeking agreement for its removal from this report. Appendix 3 is one of several Council documents that may be used for seeking contributions for specific schemes (for example, during the planning process for a new development).
- 3.4. All appendix documents contain some categorised commentary around each scheme/request, providing some contextual background information such as high-level feasibility, casualty data and, in some cases, indicative costs.

Until a scheme is fully investigated, designed and quotes have been received from appropriate contractors, it is not possible to provide detailed cost estimates, but those contained in the report reflect officer experience and a desktop review, unless otherwise stated.

There can be many legislative and physical aspects that can influence the feasibility of a scheme and the resources required to investigate requests and develop designs will incur costs. For this reason, it is not intended that any request is investigated further until funding has been identified and the Sub-Committee is asked to note that no item on this list is guaranteed as being deliverable.

Options Proposed

- 3.5. Appendix 1 provides the list of requests that are new to this update report. Members are asked to consider the recommended action for each scheme and agree the outcome as follows:
- Retain – These items will be added to the principal list (Appendix 3), awaiting funding for further investigation and development.
 - Remove – These items will be removed from the list and will not be retained for further investigation and development. The reason for this recommendation will be given.

- 3.6. Appendix 2 provides a list of requests that have formerly been part of the principal list (Appendix 3), but where Officers are making an amended recommendation. The recommendation and reason will be given.

This Appendix will also be used where a scheme has received funding for development, where a recommendation will be made for the scheme's removal from future update reports – scheme development will be reported separately.

Other Options Considered

- 3.7. None at this time.

4. Contribution to Strategic Aims

- 4.1. The Council's new Corporate Plan has established three themes for the years 2022/25. These themes are:
- Healthy Environment
 - Thriving Communities
 - Inclusive Economy
- 4.2. These themes are underpinned by "Our Foundations" explaining the ways we work at the Council:
- People first
 - Digital transformation
 - Building self-reliance
 - Getting the best value
 - Collaborating with others
- 4.3. Full details of the Council's Corporate Plan and the projects which will deliver these priorities are published on the [Council's website](#). These priorities and the Corporate Plan demonstrate how the Council meets its legal obligation to be efficient, effective and economical.
- 4.4. The recommendations of this report support the recording of a range of requests for new traffic management measures and do not directly deliver changes. Many of the requests will contribute to the Strategic Aims of the Council and, once funding becomes available, they can be developed and separately reported in greater detail.

5. Environmental and Climate Implications

- 5.1. The Council declared a Climate Emergency at its meeting on 26 February 2019 (Minute 48 refers).
- 5.2. The recommendations of this report will not directly lead to the introduction of any physical changes. As a result, an Environmental Impact Assessment has been conducted, which shows a net 'NIL' impact as a result of the Sub-Committee agreeing to the recommendation of this report.
- 5.3. Further assessments will be conducted when funding for scheme development and delivery is identified.

6. Community Engagement

- 6.1. This report records requests for traffic management measures that have been received through engagement between the Council and the community.
- 6.2. When funding becomes available for the delivery of schemes on this report, officers will engage with ward Councillors, who will also have an active role in community engagement.

- 6.3. Development of many of these requests will require statutory consultation and/or public notification. Statutory consultation will be conducted in accordance with appropriate legislation. Notices will be advertised in the local printed newspaper and will be erected on lamp columns within the affected area.
- 6.4. Traffic Management Sub-Committee is a public meeting. The agendas, reports, meeting minutes and recordings of the meetings are available to view from the Council's website.

7. Equality Implications

- 7.1. Under the Equality Act 2010, Section 149, a public authority must, in the exercise of its functions, have due regard to the need to—
- eliminate discrimination, harassment, victimisation and any other conduct that is prohibited by or under this Act;
 - advance equality of opportunity between persons who share a relevant protected characteristic and persons who do not share it;
 - foster good relations between persons who share a relevant protected characteristic and persons who do not share it.
- 7.2. It is not considered that an Equality Impact Assessment is relevant at this time as the report does not directly recommend any physical changes to the Highway. Assessment will be considered once funding for development and delivery of this scheme is identified.

8. Other Relevant Considerations

- 8.1. None arising from the recommendations of this report.

9. Legal Implications

- 9.1. None arising from the recommendations of this report.

10. Financial Implications

- 10.1. None arising from the recommendations of this report.

11. Timetable for implementation

- 11.1. It is not possible to provide an implementation timetable for the requests made in this report. When funding is identified for scheme delivery, the Sub-Committee will receive separate scheme-specific reports to provide detail around implementation timetables.

12. Background Papers

- 12.1. There are none.

Appendices

1. Appendix 1 – Provides the list of requests that are new to this update report with initial Officer comments and recommendations.
2. Appendix 2 – Provides the list of requests that have been previously-reported, where significant amendments are proposed, with Officer comments and recommendations.
3. Appendix 3 – For information. Provides the principal list of requests, as updated following the previous report to the Sub-Committee in March 2023. It also contains the prioritised list of cycling and walking measures from the LCWIP.

REQUESTS FOR TRAFFIC MANAGEMENT MEASURES, APPENDIX 1

New requests for potential entry onto the principal list, following last reported update (March 2023)

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
1	Caversham	Pedestrian crossing	Peppard Road	Near the Chiltern nursery	Request from Ward Councillor for a pedestrian crossing to make it safer for pedestrians to cross. Concerns raised about vehicles speeding in the area making it difficult to cross.	<ul style="list-style-type: none"> • Comment: The grass verge on the west side of this road, as well as the trees (roots and canopy cover) will create challenges to installing a zebra crossing in this area, but there may be a feasible location nearby. • Casualty Data: No accidents reported in this area during the latest 3 year period (up to 30th April 2023). • Anticipated Costs: A detailed investigation would need to take place before costs can be estimated. A high-level estimate for a 'standard' zebra crossing is £70k and there will be additional engineering work required to create a footway link at this location. • Officer recommendation: Retain
2	Coley	20 zone with traffic calming	Edenham Crescent	Entire road	Request from neighbourhood watch group for a 20 zone with road humps to help reduce vehicle speeds, which seems to have increased recently.	<ul style="list-style-type: none"> • Comment: Officers recommend that nearby roads be added to the 20mph scheme in order to create an area wide zone. • Casualty Data: No accidents reported in this area during the latest 3 year period (up to 30th April 2023). • Anticipated Costs: A detailed investigation would need to take place before costs can be estimated, and consideration of the appropriate area to cover. • Officer recommendation: Retain
3	Emmer Green	20mph zone	Courtenay Drive	Entire road	Request from MP to install a 20mph speed limit in Courtenay Drive.	<ul style="list-style-type: none"> • Comment: Being a potential through-road, with other roads meeting it along its length, officers recommend that nearby roads be included in this scheme, in order to create an area wide 20 zone. • Casualty Data: No accidents reported in this area during the latest 3 year period (up to 30th April 2023). • Anticipated Costs: A detailed investigation would need to take place before costs can be estimated, and consideration of the appropriate area to cover. • Officer recommendation: Retain

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
4	Katesgrove and Redlands	Traffic calming	Northumberland Avenue	Between George Palmer Close and Hexham Road	Requested via Councillor. Request for additional 20mph roundels and any other measures that could be effective as residents in this area frequently report that vehicles are speeding. Some residents have complained about noise and vibrations caused by the existing speed humps so there is no desire for additional humps.	<ul style="list-style-type: none"> • Comment: This area is part of an existing 20mph zone and has some traffic calming features. Adding vertical features could be challenging as much of this stretch is nearly at-grade with the footways. • Casualty Data: No accidents reported in this area during the latest 3 year period (up to 30th April 2023). • Anticipated Costs: Additional signs and road markings could be install at relatively low cost as this is a short section of road. Should there be a desire for more significant alterations such as humps then this would significantly increase the cost of installation and would also likely require statutory consultation. <p>• Officer recommendation: Retain</p>
5	Kentwood and Norcot	Pedestrian crossing	Norcot Road	Near its junction with Romany Lane	Request from Ward Councillor for a pedestrian crossing.	<ul style="list-style-type: none"> • Comment: The narrow footpath on one side of the road, along with the bus stop and nearby parking spaces and the junction will make it challenging to install a zebra crossing in this area so a detailed investigation needs to take place to see if it is feasible, and whether feasible locations would be at a crossing desire line. • Casualty Data: 1 slight accident reported in the latest 3 year period (up to 30th April 2023). Pedestrians were not involved in the incident. • Anticipated Costs: A detailed investigation would need to take place before costs can be estimated. <p>• Officer recommendation: Retain</p>
6	Park	Pedestrian crossing	Palmer Park Avenue	Near the Church	Request from Ward Councillor for a pedestrian crossing.	<ul style="list-style-type: none"> • Comment: There is currently an informal crossing at the junction with Wokingham Road. This would be the only location for an upgrade to a controlled crossing but a detailed investigation must take place to see if this is feasible. • Casualty Data: No accidents reported in this area during the latest 3 year period (up to 30th April 2023). • Anticipated Costs: A detailed investigation would need to take place before costs can be estimated. <p>• Officer recommendation: Retain</p>

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
7	Redlands	No entry	De Beauvoir Road	At its junction with Carnarvon Road	Request from Councillor to add additional signs to reinforce the existing restriction at this junction, due to reports of multiple vehicles going through it.	<ul style="list-style-type: none"> • Comment: Due to the narrow pavement it will be challenging to install additional signs here, and a statutory consultation may also be required, depending on what is installed here. • Casualty Data: No accidents reported in this area during the latest 3 year period (up to 30th April 2023). • Anticipated Costs: A detailed investigation would need to take place before costs can be estimated. • Officer recommendation: Retain
8	Redlands	Alterations to existing, and additional traffic calming features	Redlands Road, Morgan Road, Alexandra Road	Entire length of roads	Requests for changes, including walkout, summarised in report to TMSC in June 2023.	<ul style="list-style-type: none"> • Comment: Primary change is the removal of the priority flow feature, Redlands Road jcn Allcroft Road. Agreement needed about the replacement feature. Other requested changes include: Replacement of Redlands Road speed cushions with tables, replacement of priority flow between Upper Redlands Road and New Road (feature TBA), removal of build-outs between Addington and Allcroft Roads, entrance treatments for side roads off Redlands Road and reprofiling of speed humps on Allcroft and Morgan Roads. • Casualty Data: 2 serious & 3 slight casualty incidents during the latest 3 year period (up to 30th April 2023). Various factors, all Redlands Road and none specifically attributed to speeding. • Anticipated Costs: A detailed investigation would need to take place before costs can be estimated and there be a consensus/agreement regarding the desirable replacement features, the suitability and feasibility of these. TMSC November 2022 report estimated c.£15k at the time for replacing the Redlands/Allcroft Road priority flow feature with speed cushions. • Officer recommendation: Retain

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
9	Southcote	Traffic calming	Kenilworth Avenue & Monks Way	At their junctions with Southcote Lane	Request from Ward Councillor to install a raised table at these junctions similar to the one on Fawley Road/Southcote Farm Lane. Petition at TMSC September 2023 to request the same. Offer of support through potential financial and/or land contribution.	<ul style="list-style-type: none"> • Comment: Southcote Lane is part of an existing 20mph zone, however, these streets are not adopted Highway. A petition report to September 2023 TMSC provided some options and challenges, which may include the need to adopt some of the private land to make a scheme deliverable. • Casualty Data: No accidents reported in this area during the latest 3 year period (up to 30th April 2023). • Anticipated Costs: A detailed investigation would need to take place before costs can be estimated and costs will vary dependant on the necessity and extent of Highway adoption. • Officer recommendation: Retain
10	Thames	Pedestrian crossing	Kenavon Drive	Near its junction with Forbury Road	Request from resident for a formal crossing at the junction. Visually impaired pedestrians are struggling to cross this section safely.	<ul style="list-style-type: none"> • Comment: Such a facility would need to be set back further than the existing traffic island in order to ensure there is adequate time for motorists to slow down when pedestrians are waiting to cross. Feasibility, from a safety perspective, may be challenging due to the proximity of junctions in this section of the road. • Casualty Data: No accidents reported in this area during the latest 3 year period (up to 30th April 2023). • Anticipated Costs: A detailed investigation would need to take place before costs can be estimated. • Officer recommendation: Retain
11	Thames	Road Closure (Relocation)	Milford Road	Southern end of street	Requested via Ward Councillor. Request to relocate the current road closure near the junction with Cardiff Road, to the northern side of Printers Way. Printers Way has been severed by the developer and residents wish to be able to enter/exit the western side via Cardiff Road, which they currently cannot.	<ul style="list-style-type: none"> • Comment: The relocation of the road closure will require a statutory consultation which may result in objections from residents. • Casualty Data: No accidents reported in this area during the latest 3 year period (up to 30th April 2023). • Anticipated Costs: A detailed investigation would need to take place before costs can be estimated. • Officer recommendation: Retain

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
12	Tilehurst	20mph zone	Westwood Glen	Entire road	Request from Ward Councillor for a 20mph zone in Westwood Glen.	<ul style="list-style-type: none"> • Comment: Officers recommend that nearby roads be included in this scheme, in order to create an area wide 20 zone. • Casualty Data: No accidents reported in this area during the latest 3 year period (up to 30th April 2023). • Anticipated Costs: A detailed investigation would need to take place before costs can be estimated, and consideration of the appropriate area to cover. • Officer recommendation: Retain

This table is arranged by Ward (A-Z), then by Street (A-Z)

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REQUESTS FOR TRAFFIC MANAGEMENT MEASURES, APPENDIX 2

Proposed amendments to requests from the principal list (Appendix 3), since last reported update (March 2023)

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
1	Abbey	Signing	Abbey Square	Entire road	<p>Complaint from resident. Cars coming out the back of the Forbury Hotel often turn left out of the driveway and go the wrong way.</p> <p>Amendment (November 2023): Proposed removal following agreement at TMS September 2023 to proceed with the scheme at Jacksons Corner. This will reverse the one-way restriction and will be signed/lined accordingly.</p>	<ul style="list-style-type: none"> • Comment: Built-out alterations and/or signing and lining adjustments may reduce these instances. • Casualty Data: No incidents in the latest 3 year period of data (up to August 2021). • Anticipated Costs: A very high level estimate would be £8,000 for minor changes to lines and signs, but build-out/kerbing alterations will be considerable additional costs, depending on the application. • Officer recommendation: Remove. Jacksons Corner scheme will remove the need for this entry.
2	Abbey	Cycle Signing	Various	Town centre	<p>Review town centre signing and update to ensure compliance with TSRGD. Locations include: Queen Victoria Street Market Place Town Hall Square</p> <p>Amendment (November 2023): Proposed removal as this work is considered to have been undertaken in Summer 2023 and there is ongoing engagement with the Cycle Forum.</p>	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum. • Casualty Data: N/A • Anticipated Costs: A high level estimate would be around £3000. • Officer recommendation: Remove. This work is considered to have been completed

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
3	Caversham Heights	Traffic calming, pedestrian crossing and footway improvements	Kidmore Road	Between its junctions with Highmoor Road and Shepherds Lane, sitting alongside separate request for the section south of Highmoor Road.	<p>Request to install traffic calming features along this road and to make improvements to the footway to prevent pedestrians from having to walk in the road or cross. A crossing (formal or informal) would also be beneficial close to the Richmond Road junction. These would improve access and safety for pedestrians, including school children who walk through the area.</p> <p>Amendment (November 2023): This entry has been extended from Richmond Road to Highmoor Road and, alongside a separate entry for the section south of Highmoor Road, is intended to capture the local desire for speed calming treatment along the entire length of the street.</p>	<ul style="list-style-type: none"> • Comment: A detailed investigation will be required to determine the scope of the footway work and feasibility of a crossing at the requested location. Traffic calming in a 30mph area would also require costly illuminated signs, so 20mph could be a consideration. • Casualty Data: 2 slight accidents reported in the latest 3 year period (up to 30th April 2023). Both near the Oakley Road junction. Speed was considered a contributing factor for both of these incidents. • Anticipated Costs: A detailed investigation would be required before costs can be estimated. • Officer recommendation: Retain with amendments
4	Katesgrove	20mph with traffic calming	Alpine Street, Francis Street and Edgehill Street	Entire Roads	<p>Request for a 20mph zone with traffic calming such as speed humps in order to reduce vehicle speeds.</p> <p>Francis Street added, following complaints of speeding and parked vehicle damage.</p> <p>Amendment (November 2023): Petition received at September 2023 TMS for traffic calming measures, and measures to reduce traffic volumes on Francis Street. 32 signatures.</p>	<ul style="list-style-type: none"> • Comment: Officers recommended that Edgehill Street also be included as part of the original Alpine Street request. This is developing into a wider area scheme that could incorporate the streets between Elgar Road, Pell Street and Southampton Street. Speed surveys should also be carried out to assess vehicle speeds before determining what type of traffic calming features would be appropriate. • Casualty Data: No incidents reported in the latest 3 year period (up to August 2021). • Anticipated Costs: A very high level estimate would be around £70,000. • Officer recommendation: Retain with amendments

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
5	Multiple: Abbey / Thames	Cycle Access	Reading Station Subway	Subway	<p>Request to allow cycling along the station subway.</p> <p>Amendment (November 2023): Proposed removal as this work will be undertaken following agreement by TMSC in March 2023 and following the forthcoming ceiling improvement works.</p>	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum and an action to capture requests made by the forum that are not already on the main list of requests. • Casualty Data: N/A - relates to improved access. • Anticipated Costs: Proposals are being developed. • Officer recommendation: Remove. Implementation of the works has been agreed.
6	Multiple: Caversham Heights / Emmer Green	Pedestrian Crossing and Traffic Calming measures	Rotherfield Way	<p>Pedestrian crossing - South-west of its junction with Surley Row</p> <p>Traffic calming - entire length</p>	<p>A petition to install 'safe crossing places' on Rotherfield Way was reported to Jan 2016 TMSC. An update report went to March 2016 TMSC. A further update report (with an outline zebra crossing design) was reported to June 2016 TMSC.</p> <p>Amendment (November 2023): TMSC agreed in March 2023 that this request be amended to include traffic calming following a new petition that was received. The petition highlighted resident's concerns about speeding in the area, alongside a desire for traffic calming features and a pedestrian crossing near Surley Row.</p>	<ul style="list-style-type: none"> • Comment: A concept scheme is awaiting funding to enable it to progress to detailed design and implementation for a zebra crossing. Ground investigation works will determine the deliverability of the crossing. A number of different traffic calming features can be explored, however, if the speed limit is 30mph, then costly illuminated signs will also need to be installed if features such as humps are installed on the road. 20mph could be a consideration. • Casualty Data: No incidents recorded in the latest 3 year period of data (up to 30th April 2023). • Anticipated Costs: A high level estimate would be £80,000 for a zebra crossing. The cost of traffic calming features will vary greatly depending on whether or not the road remains 30mph, and what type of features are installed. • Officer recommendation: Retain with amendments

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
7	Park	Bollard	Green Road	At the closure point	<p>Request received to install additional bollard, or redistribute existing bollards at the closure point, as vehicles are reportedly using the dropped pedestrian kerb to negotiate the closure.</p> <p>Amendment (November 2023): Proposed removal as this work is considered to have been undertaken in Summer 2023.</p>	<ul style="list-style-type: none"> • Comment: It may be possible to reduce the gap(s) to prevent cars and vans from being able to squeeze past the closure, but the feature needs to remain accessible for mobility aids and pushchairs etc., so could still be open to potential abuse by smaller motorised vehicles. We also need to ensure that any proposal doesn't create additional obstacles for those with impaired vision. • Casualty Data: No incidents recorded in the latest 3 year period of data (up to August 2021). • Anticipated Costs: A high level estimate would be £1000 for one bollard but more if we change existing ones. • Officer recommendation: Remove. This work is considered to have been completed
8	Park	20mph enhancements	Newtown area	Entire area, in particular Coventry Road, Cholmeley Road and Amity Street	<p>Officers have received a request for additional physical speed calming measures and repeater signs for the 20mph limit along Coventry Road, a request for 20mph repeaters such as signs or roundels along Cholmeley Road and a request for a physical traffic calming measure such as a speed hump on the west end of Amity Street. These would remind motorists of the existing 20mph speed limit in this area and may also reduce the number of vehicles being damaged.</p> <p>Amendment (November 2023): This entry has been amended to include requests for Cholmeley Road and Amity Street.</p>	<ul style="list-style-type: none"> • Comment: Speed surveys should be carried out to assess vehicle speeds to determine which areas in Newtown could benefit the most from additional calming measures. There is scope for additional speed humps and for repeater signs to improve speed compliance, although it should be noted that these will likely not eradicate the issues raised for those who are already wilfully driving inconsiderately. • Casualty Data: No injury related accidents in the latest 3 year period (up to 30th April 2023) where speeding was considered a contributing factor. • Anticipated Costs: A very high level estimate would be £55,000 but could increase significantly depending on the number (and type) of traffic calming features installed. • Officer recommendation: Retain with amendments

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
9	Redlands	Pedestrian Crossing	Upper Redlands Road	Near to St Josephs College and at junction with Alexandra Road.	<p>Request received for improved pedestrian crossing facilities to the east of Alexandra Road. Suggestion made for turning the speed cushions into a full-width raised crossing (with imprinting on top), although a controlled crossing is preferred. Also requested improvements at the junction with Alexandra Road to improve the crossing for pedestrians and to reduce the carriageway with the intention of reducing vehicle speeds.</p> <p>Amendment (November 2023): Proposed removal from this list, following report to TMSC in September 2023 confirming that this scheme is now funded.</p>	<ul style="list-style-type: none"> • Comment: A concept scheme has been developed and there has been some engagement with the University of Reading and St Josephs College regarding this design, which locates the crossing near to the junction with Alexandra Road. Fundraising has raised some private local funding commitments for developing the proposal. • Casualty Data: 3 slight incidents reported at the junction of Alexandra Road/Upper Redlands Road in the latest 3 year period (up to 30th April 2023). None involving pedestrians. • Anticipated Costs: A high level estimate would be £60,000 for a 'standard' zebra crossing. • Officer recommendation: Remove Funding has been secured from a successful bid to government for Active Travel and the scheme is now being developed.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
10	Tilehurst	Speed calming and traffic management measures Amendment (November 2023): Pedestrian Crossing	Conwy Close	Entire length	Request from parent whose child attends the Avenue School, for road safety measures such as signs, lines, traffic calming and/or a pedestrian crossing to improve safety at this location. There are concerns about safety due to the high volume of vehicles and pedestrians that use this road e.g. taxis and minibuses parking on the pavement, double parking and general traffic build up. Amendment (November 2023): Request from Ward Councillor for a pedestrian crossing.	<ul style="list-style-type: none"> • Comment: The installation of traffic calming could result in noise complaints and will be costly. It may be beneficial to conduct a speed survey to assess vehicle speeds and investigation is needed to determine what measures could be appropriate here. It may also be worth considering a 20mph zone in the road. Provision of a controlled crossing will be subject to feasibility. It would need to be set back from the junction with The Meadway to ensure acceptable intervisibility, but also away from driveway and car park accesses. • Casualty Data: No accidents reported in this area during the latest 3 year period (up to 30th April 2023). • Anticipated Costs: A high level estimate would be around £50,000 to implement a 20mph zone with traffic calming. A 'standard' zebra crossing is estimated to cost £70k, subject to feasibility and additional engineering challenges. • Officer recommendation: Retain with amendments

REQUESTS FOR TRAFFIC MANAGEMENT MEASURES, APPENDIX 3

Principal list of requests, as updated following the previous report to the Sub-Committee in March 2023.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
1	Abbey	Pedestrianisation (motor vehicle prohibition)	Abbots Walk	Eastern end, beyond the turning head and rear access to numbers 10-12.	Requested via Ward Councillors. Request to pedestrianise this section of the carriageway as there is no vehicular access beyond this point, however, there are parking and vehicle movement issues being experienced. This is partly attributed to a suspicion that motorists believe that there is vehicular access beyond Abbots Walk.	<ul style="list-style-type: none"> • Comment: A motor vehicle prohibition TRO would require advertising, implementing and signing, alongside a separate TRO for parking restriction alterations. It is recommended that the prohibition be set back from the turning head, to facilitate safe turning for vehicles accessing up to this point. It is recommended that the closure has physical measures to ensure compliance (e.g. lockable bollards). • Casualty Data: No incidents involving casualties recorded in the latest 3 year period of data (up to end of September 2022). • Anticipated Costs: A high-level estimate is £15k to cover TRO work, a single illuminated regulatory sign and bollards to prevent vehicular access.
2	Abbey	Pedestrian crossings	Bridge Street	Outside the Civic Offices	Request from resident to upgrade the existing traffic island with imprint to a full zebra crossing due to concerns about pedestrian safety.	<ul style="list-style-type: none"> • Comment: A detailed investigation would need to be carried out to determine the feasibility of a crossing at this location, as it is not likely that a crossing can be installed anywhere else in this area. • Casualty Data: 2 slight incidents reported in the latest 3 year period (up to end May 2022), involving vehicles turning out of Fobney Street. Pedestrians were not involved in either of these incidents. • Anticipated Costs: If a controlled crossing can be installed, a very high level estimate would be around £80,000, but could be considerably higher depending on any special engineering requirements. Detailed investigation is required.
3	Abbey	Cycle Access	Cheapside	Cheapside/Friar Street	Allow right turn from Cheapside onto Friar Street	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum. This would require a TRO change, lining and signing (including de-illumination) alterations. However, alterations to the pedestrian island are also recommended for consideration, to change the shallow angle that traffic turning right onto Cheapside is currently taking and to reduce the risks to cyclists (and other vehicles) waiting to turn right onto Friar Street. Visibility checks and a road safety audit would be necessary. • Casualty Data: N/A - this request relates to increased access. • Anticipated Costs: A high level estimate would be around £8,000 + any necessary alterations to the island.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
4	Abbey	Cycle Access	Friar Street East	Between Queen Victoria Street & Station Approach, including Blagrove Street	Contraflow cycle facilities to allow two-way cycle flows through the town centre	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum. The pressure on kerb space within the town centre (including bus, taxi, loading facilities), significant pedestrian flows and the bend in the road make this a challenging proposal to deliver. • Casualty Data: N/A - this request relates to increased access. • Anticipated Costs: A detailed investigation would be required before costs can be estimated as it would depend on the extent of the scheme.
5	Abbey	Cycle access	Great Knollys Street	Entire street	Advance stop line for Great Knollys Street junction	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum. • Casualty Data: N/A • Anticipated Costs: A detailed investigation would be required before costs can be estimated as it would depend on what measures can be installed.
6	Abbey	Cycle access	Kings Road	Junction with Watlington Street	Provide advance stop line at bus lane on Kings Road / Watlington Street.	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum. This will likely require alterations to traffic signal detection equipment and configuration. • Casualty Data: 1 serious and 2 slight accidents reported in this area in the latest 3 year period (up to August 2021). 1 slight incident involving a pedal cycle. • Anticipated Costs: A high level estimate would be around £10,000 if alterations are needed to the detection.
7	Abbey	Cycle Access	Market Place	Between Kings Road and Town Hall Square	Contraflow cycle facilities to allow two-way cycle flows through the town centre	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum. • Casualty Data: N/A - this request relates to increased access. • Anticipated Costs: A detailed investigation would be required before costs can be estimated as it would depend on what measures can be installed given the narrow width of Market Place and the pedestrian flows/cafe usage along Butter Market.
8	Abbey	Cycle Access	Minster Street	Minster Street/Yield Hall Place	Improved access from Minster Street to Oracle Riverside	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum. • Casualty Data: N/A - this request relates to increased access. • Anticipated Costs: A detailed investigation would be required before costs can be estimated as it would depend on what measures can be installed.
9	Abbey	Cycle Access	Oxford Road	Oxford Road linking to Hosier Street via Queens Walk	Improved access to shared-use facilities via dropped kerb as full height kerb currently in place	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum. • Casualty Data: N/A - this request relates to improved access. • Anticipated Costs: A high level estimate would be around £3000.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
10	Abbey	Cycle Access	Southern Interchange	Garrard Street / Stanshawe Road / Southern Interchange	Improved access and signing to/from Garrard Street and Stanshawe Road junctions to Southern Interchange	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum. It is anticipated that local redevelopment will lead to the delivery of cycle infrastructure on Greyfriars Road and Garrard Street. This request will remain on the list until this is confirmed/delivered. • Casualty Data: 1 slight incident reported in the latest 3 year period (up to August 2021) but not involving a pedal cycle. • Anticipated Costs: This is expected to be delivered as part of local development works.
11	Abbey	Cycle Signing	Various	Town centre	Improved clarity of cycle routes in town centre	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum. • Casualty Data: N/A • Anticipated Costs: A high level estimate would be around £3000.
12	Abbey	Cycle Parking	Various	Various	Additional cycle parking at key points in the town centre. For example: St Mary's Butts, Station Road, Cross Street and Hosier Street.	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum. • Casualty Data: N/A • Anticipated Costs: A detailed investigation would be required before costs can be estimated as it would depend on what measures can be installed.
13	Abbey	Cycle access	Various linked to Abbey Quarter Development		Improve cycling facilities into/from/through Abbey Quarter development site	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum. • Casualty Data: N/A - this request relates to improved access. • Anticipated Costs: A detailed investigation would be required before costs can be estimated as it would depend on what measures can be installed.
14	Battle	One way plug	Connaught Road	At its junction with Oxford Road	Request from residents to make this road one way due to issues caused by motorists refusing to give way.	<ul style="list-style-type: none"> • Comment: A detailed investigation will be required to determine the full impact of changes to this area and the feasibility of any physical measures that would be installed to prevent traffic from turning left into Connaught Road from Oxford Road. • Casualty Data: 2 slight incidents reported in the latest 3 year period (up to end May 2022) involving a vehicle turning out of Connaught Road and another where a vehicle reversed into Connaught Road from Oxford Road. No pedestrians were involved in these incidents. • Anticipated Costs: A detailed investigation would be required for the preferred solution before costs can be estimated.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
15	Battle	Pedestrian Crossings	Portman Road	East of Tesco and also near Bridgewater Close	Request for pedestrian crossings as traffic levels have increased on this road in 2020, making it harder for pedestrians to cross to access the industrial estate.	<ul style="list-style-type: none"> • Comment: There is likely to be some funding contribution toward measures through Section 106 contributions. The area will need to be reviewed to determine the best location for a crossing. This is particularly the case to find a good and suitable crossing link near to Bridgewater Close. • Casualty Data: One serious accident at the junction with Little John's Lane in the latest 3 year period (up to August 2021). No pedestrians involved. No accidents reported in the immediate area around Tesco or Bridgewater Close. • Anticipated Costs: A very high level estimate would be around £70-80k for one zebra crossing, due to the additional footway links that will be required across the verges. This could be significantly higher near Bridgewater Close, depending footway links on the southern side of the road.
16 Page 54	Caversham	Pedestrian Crossing	Briants Avenue	Near to South View Avenue	Local resident requested formal crossing (e.g. zebra) to ease the crossing of Briants Avenue. There is no controlled pedestrian crossing along Briants Avenue.	<ul style="list-style-type: none"> • Comment: A detailed investigation would be needed to ascertain what features can be installed. It is not at all likely that a zebra crossing can be installed in this area due to the visibility issues caused at the bend in the road, the number of dropped kerbs, junctions and the bus stop and it would also require the removal of a number of parking spaces. This would move the crossing further down the road, away from this desire line and would still require some parking removal. • Casualty Data: 1 slight and 1 serious incidents reported in the latest 3 year period (up to August 2021). These were at the southern end of Briants Ave but the serious incident involved a pedestrian crossing the road. • Anticipated Costs: Survey: A detailed investigation would be required before costs can be estimated as it would depend on what features can be installed. If a suitable and likely useful position can be found, a typical zebra crossing could cost around £60k-80k depending on the location and the level of works required.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
17	Caversham	Pedestrian Crossing	Bridge Street	Junction of Bridge Street, Church Street and Church Road	Petition received at November 2017 TMSC for the installation of controlled pedestrian crossing facilities at this junction.	<ul style="list-style-type: none"> • Comment: The petition update report at Jan 2018 TMSC noted the challenges in implementing this facility within the traffic signal controlled junction and the need for traffic impact modelling, which will require external expertise. • Casualty Data: 4 slight incidents reported in the area in the latest 3 year period (up to August 2021). None involving pedestrians. • Anticipated Costs: A detailed investigation would be required before costs can be estimated as it would depend on what features can be installed.
18	Caversham	20mph & speed calming	Chiltern Road	Whole length, but officers would recommend inclusion of streets linked from Chiltern Road, to achieve a cohesive scheme.	Request for 20mph and speed calming due to reported vehicle damage caused by vehicles driving inappropriately fast.	<ul style="list-style-type: none"> • Comment: It would be advisable to consider the wider area and not just this road in isolation. Speed surveys should be carried out to assess vehicle speeds before deciding where traffic calming features should be used. • Casualty Data: No injury related accidents reported in the latest 3 year period (August 2021). • Anticipated Costs: A very high level estimate would be £60,000.
19	Caversham	Lining alterations	Church Street	At its junction with Hemdean Road	Request to review the lining on Church St to help reduce traffic build up caused by vehicles trying to turn right into Hemdean Road. A right turn filter lane was suggested.	<ul style="list-style-type: none"> • Comment: A detailed investigation will be required to determine what type of changes could be made to this junction. It is possible that this will not be feasible, given the width of road available. • Casualty Data: 1 slight incident reported in the latest 3 year period (up to end May 2022) involving a pedestrian crossing near the garage. • Anticipated Costs: A full investigation will need to be made to determine whether or not any lining changes can be made to improve traffic at this location.
20	Caversham	Zebra crossing	Hemdean Road	Near Caversham Primary School	From Councillors on behalf of resident and school. More children are coming to the school through Balmore Park and there are concerns about safety risks when crossing Hemdean Road to access the school.	<ul style="list-style-type: none"> • Comment: Some feasibility concerns at this location. The bus stop would need to move, which could be challenging in terms of avoiding visibility issues at the crossing. Speed cushions would likely need to be removed, but potential to locate new ones nearby. The parking outside the school needs to be removed (being considered as part of a Waiting Restriction Review Programme). There are two desire-lines for different aged pupils, so precise positioning will need to be considered. • Casualty Data: No incidents reported in the latest 3 year period (up to end May 2022) between its junctions with Hemdean Hill and Grove Hill. • Anticipated Costs: If a controlled crossing can be

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
						installed, a very high level estimate would be around £100,000, but could be considerably higher depending on any special engineering requirements. Detailed investigation is required.
21	Caversham	Pedestrian Crossing	Henley Road	Junction of Henley Road, Peppard Road, Prospect Street and Westfield Road	<p>Petition received at November 2017 TMSC for the installation of controlled pedestrian crossing facilities at this junction. Another petition was received for this crossing in Oct 2022 with 1341 responses, reported to TMSC in November 2022.</p> <p>Amendment (for March 2023): A petition was reported to TMSC in November 2022, containing the results of a wider area survey undertaken by the local MP. The survey showed 1244 respondents in favour of a pedestrian crossing at this junction. There has been additional correspondence suggesting options, such as placement of crossings further back from the junction and an interim/lower-cost option of providing a pedestrian refuge island on the Henley Road approach, which the petition report makes reference.</p>	<ul style="list-style-type: none"> • Comment: The petition update report at Jan 2018 TMSC and November 2022 TMSC note the challenges in implementing this facility and other suggested options within the traffic signal controlled junction. • Casualty Data: 1 slight and 1 serious incident involving pedestrians in the latest 3 year period (up to the end of September 2022). • Anticipated Costs: A detailed investigation would be required before costs can be estimated as it would depend on what features can be installed. The preferred option would involve a complete technical refresh and replacement of the signal equipment and associated engineering works.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
22	Caversham	Banned Vehicle Movement	Peppard Road	Junction with Derby Road	<p>Councillor has reported resident concerns about the volume of traffic entering Derby Road (a private Road), particularly around school drop-off/pick-up times, then conducting turns in the road to then leave.</p> <p>They feel that a 'no-left-turn' restriction on Peppard Road, with appropriate exemptions for residents, would reduce these occurrences.</p>	<ul style="list-style-type: none"> • Comment: This entry was agreed for retention by TMSC (Sept 2019). Such restrictions require a Traffic Regulation Order to have been formally, publicly, consulted and implemented. The allowable exemption sign would state 'Except authorised vehicles', with no reference to residents being permissible. The authorised vehicles would be defined in the TRO (e.g. vehicles belonging to residents and their visitors). The sign(s) would require illuminating. The restriction would typically be used to benefit traffic flow on the main road, which it would not likely achieve in this application. Enforcement of this restriction is likely to be the primary deterrent. It is expected that Civil Enforcement powers will be provided to local authorities from summer 2022, until which time only the Police can enforce the restriction. Camera enforcement will be very costly, however. • Casualty Data: No incidents recorded in the latest 3 year period of data (up to August 2021). • Anticipated Costs: A high level estimate would be £15,000 for the restriction, assuming 2 illuminated signs but not for enforcement.
23	Caversham	20mph	Various	Amersham Road area	<p>A report to Sept 2016 TMSC proposed a 20mph zone that could cover the Lower Caversham and Amersham Road estate areas. This report was the result of a number of petitions and requests for 20mph in these areas. It was agreed that there would need to be further consultation with Councillors and CADRA, but noted that there was currently no funding for the scheme.</p> <p>At the request of Caversham Ward Councillors, this amendment for November 2022 separates out the Amersham Road area from the remainder of the proposal.</p>	<ul style="list-style-type: none"> • Comment: It has been requested that the Amersham Road area be separated from the wider Lower Caversham area for reporting. Speed surveys would be beneficial to see if the area could benefit from additional traffic calming measures, or to amend existing ones which are already in place. • Casualty Data: 1 slight incident reported in the latest 3 year period (up to end May 2022) on Amersham Road but speeding was not considered a contributing factor. • Anticipated Costs: Provided that there is no desire to amend the existing traffic calming features in the area, the installation of signs, lines and calming features on Ian Mikardo Way and Charles Evans Way would be estimated at around £30,000. The costs would increase significantly should there be a desire to amend any existing features.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
24	Caversham Heights	Speed calming	Albert Road	Entire Length	Requested by Ward Councillor and follows a wide area survey of over 180 responses, undertaken by MP. 23 people raised Albert Road as their greatest area road safety concern with 16 supporting 20mph. It is a designated local cycling route on LCWIP.	<ul style="list-style-type: none"> • Comment: 20mph will require a robust set of physical features for compliance (making the restriction 'self-enforcing'). This comes with compromises, such as potential increases in road noise. This request also needs to be considered in the context of other requests in the area and adjacent streets/links should also be considered, which will significantly increase the area and resultant scheme costs. Speed calming and 20mph may create a beneficial reduction in the reported rat-running and overall traffic volumes. • Casualty Data: 1 'slight' incident involving casualties recorded in the latest 3 year period of data (up to end of September 2022). Not specifically attributed to speeding. • Anticipated Costs: A high-level estimate for delivering the core elements of a 20mph zone on Albert Road (TRO, signing, lining and an assumption of speed humps or cushions) is £160k.
25	Caversham Heights	Speed calming	Conisboro Avenue	Entire Length	Requested by Ward Councillor and follows a wide area survey of over 180 responses, undertaken by MP. 12 people raised Conisboro Avenue as their greatest area road safety concern with 10 supporting 20mph. It is a designated local cycling route on LCWIP.	<ul style="list-style-type: none"> • Comment: 20mph will require a robust set of physical features for compliance (making the restriction 'self-enforcing'). This comes with compromises, such as potential increases in road noise. This request also needs to be considered in the context of other requests in the area and adjacent streets/links should also be considered, which will significantly increase the area and resultant scheme costs. Speed calming and 20mph may create a beneficial reduction in the reported rat-running and overall traffic volumes. • Casualty Data: No incidents involving casualties recorded in the latest 3 year period of data (up to end of September 2022). • Anticipated Costs: A high-level estimate for delivering the core elements of a 20mph zone on Conisboro Avenue (TRO, signing, lining and an assumption of speed humps or cushions) is £130k.
26	Caversham Heights	Signing	Conisboro Avenue / Sandcroft Road	At the bend in the road, where the streets meet.	Councillor requested, on behalf of residents, the installation of 'bend in the road' advance warning signs and a 'no through road' sign for Conisboro Avenue, to the north of this bend.	<ul style="list-style-type: none"> • Comment: Signs can be installed without illumination. • Casualty Data: No incidents reported in the latest 3 year period (up to August 2021). • Anticipated Costs: A high level estimate would be £1500.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
27	Caversham Heights	20mph zone	Tokers Green Lane	Entire Road	A request for a 20mph zone on this road, which would be a jointly managed scheme between Reading Borough Council and South Oxfordshire District Council.	<ul style="list-style-type: none"> • Comment: It would be beneficial to conduct surveys to assess vehicle speeds and appropriate measures. Speed calming devices could increase noise complaints and will be costly. Feasibility risks around drainage and risks of ponding around any physical traffic calming features. Would require support, funding and a joined-up approach with Oxfordshire County Council, as the road spans local authority boundaries. • Casualty Data: No incidents in the latest 3 year period (up to August 2021, within the Borough of Reading). • Anticipated Costs: A very high level estimate would be around £20,000, based on a contribution to Reading's section only.
28	Caversham Heights	Signing/ Lining	Upper Woodcote Road	The bend near Richmond Road	Resident has concerns about the safety of the bend near the junction with Richmond Road, stating that the police have told residents there is an adverse camber. Additional signs and refreshing existing lining could help highlight the bend.	<ul style="list-style-type: none"> • Comment: This location has a bend in the road and a junction with a right-turn filter lane. It is a wide section of road and is not significantly cambered, but is slightly barrelled across its profile. This is not a location with an evidenced road safety (casualty) issue and is not a high speed road. It is most likely that any incidents at this location are caused by motorists miss-judging their approach speed, the weather/road conditions or by intentions to cut the corner when the filter lane is being occupied. A review and potential improvement of the local warning signs and lining may be beneficial and of a relatively low cost. Lining condition is regularly inspected and will be refreshed as necessary. • Casualty Data: No incidents recorded in the latest 3 year period of data (up to August 2021). • Anticipated Costs: A high level estimate would be £2000

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
29	Caversham Heights	Speed Calming	Upper Woodcote Road and Woodcote Road	General	Request from residents for measures to be put in place to prevent speeding, such as a speed indicator device. Woodcote Road added, following additional complaints about speeding.	<ul style="list-style-type: none"> • Comment: There would need to be consideration about whether there is a desire to lower the speed limit and whether speed survey data and the Police would support this. Traffic calming can be applied to 30mph roads, but will require illuminated signing, which will considerably increase the scheme costs (est. £5k per sign). The types of traffic calming features would also be restricted as this is a nationally-classified 'A' road, with other feasibility challenges around the number of dropped vehicular crossings (driveway accesses) along the street. Following additional correspondence, Officers also recommend that Woodcote Road be considered as part of this request. • Casualty Data: 4 slight and 1 serious incidents reported in the latest 3 year period (up to end May 2022). Of these, the serious incident listed speeding as a likely causation factor (Upper Woodcote Road, close to Shepherd's lane). 1 slight incident was on Woodcote Road. • Anticipated Costs: A detailed investigation would be required before costs can be estimated as it would depend on what features are installed.
30	Caversham Heights	Pedestrian Crossing	Upper Woodcote Road (and Woodcote Road)	General	A number of requests have been made for improvements to pedestrian crossings (and increased numbers) along the street.	<ul style="list-style-type: none"> • Comment: A crossing has been installed close to its junction with Knowle Close, however, there would be benefit in considering some of the other areas that attract a higher footfall and providing appropriate facilities to assist pedestrians, expanding to Woodcote Road also. Facilities could range from informal, to controlled crossings (e.g. zebra crossings) • Casualty Data: 1 serious and 1 slight incidents reported in the latest 3 year period (up to August 2021). Of these, the serious incident listed speeding as a likely causation factor (close to Shepherd's lane). • Anticipated Costs: A detailed investigation would be required before costs can be estimated as it would depend on what features are installed.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
31	Church	Traffic calming	Buckland Road	Entire road	Request for traffic calming such as speed humps in order to reduce vehicle speeds, especially on approach to the traffic lights at its junction with Basingstoke Road.	<ul style="list-style-type: none"> • Comment: On a 30mph street there would need to be (costly) illuminated warning signs, which also carry ongoing revenue costs. Consideration should be made for making this a 20mph street instead, which would need a range of features and signing within. • Casualty Data: 6 slight and 3 serious accidents in the latest 3 year period of data (up to August 2021), with a variety of causation factors. One incident involving a pedestrian where speeding was considered a contributing factor. • Anticipated Costs: A very high level estimate would be around £60,000 for a 20mph zone with some traffic calming features.
32	Church	Traffic calming	Northumberland Avenue	Close to the junction with Stockton Road	Request for traffic calming to be considered or else remove the mini roundabout and revert it to a standard junction. Complaints received about drivers travelling too fast when approaching and manoeuvring around the junction.	<ul style="list-style-type: none"> • Comment: The issue of motorists choosing to drive at inappropriate speeds and driving across mini-roundabouts is challenging to address with physical measures, particularly considering the space constrictions, that it is a bus route and with the pedestrian facilities in this area. It is possible that an extension of the existing 20mph restriction further to the north along Northumberland Avenue could provide some benefits, with supporting traffic calming. The types of measures will need careful consideration, as this is a key public transport corridor and likely to be a useful emergency service route - simply installing lots of speed humps will not be appropriate. • Casualty Data: No incidents reported in this area in the latest 3 year period (up to August 2021). • Anticipated Costs: A high level estimate for the extension of the 20 zone down to Hartland Road with cushions would be £75,000 though this could increase depending on the area covered.
33	Church	Lining - Keep Clear	Whitley Wood Road	Junction with Tamarisk Avenue	Request received to place a keep clear marking on Whitley Wood Road to facilitate the right-turn onto Tamarisk Avenue and avoid occasional queuing back into Shinfield Road junction.	<ul style="list-style-type: none"> • Comment: This would be a low cost measure that could benefit residents and traffic flow on the main road. • Casualty Data: There have been no recorded incidents involving casualties at this junction within the latest 3 year period (up to August 2021). • Anticipated Costs: A high level estimate would be £1000.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
34	Coley	Pedestrian crossing	Bath Road	Close to its junction with Harrow Court	Request to upgrade the historic islands neat the hospital to a formal crossing to either a pelican or puffin crossing, due to concerns about safety and vehicle speeds.	<ul style="list-style-type: none"> • Comment: For safety, controlled crossings require good (and specified) advance visibility and to be away from junctions. Either side will be challenging, as there are dropped kerbs for driveway accesses, junctions close to a bus stop that may need to be relocated. While detailed investigation would be required, it may be the case that a controlled crossing is not achievable near to the desire lines but that some other enhancements or informal features may help. Additional feasibility concerns around the available carriageway width in which to alter lining and islands to create a compliant 'split' crossing. • Casualty Data: 1 slight incident near this location in the latest 3 year period (up to August 2021) where speeding was considered a contributing factor. • Anticipated Costs: A very high level estimate would be around £90,000, if a zebra crossing could be installed, taking into account island alterations and electrical works.
35	Coley	Keep Clear markings	Berkeley Avenue	Junction with its service road	Request from resident via Councillor to install a keep clear marking to stop the junction from being blocked by queueing vehicles.	<ul style="list-style-type: none"> • Comment: The correct application of these markings is to reduce delays on the primary road, caused by right-turn traffic not being able to enter the side road due to queueing traffic. If this is the intended application, and not the perception of aiding traffic turning out of the side road, the recommendation is to retain this item on the list. • Casualty Data: N/A • Anticipated Costs: A high level estimate would be £1000.
36	Coley	Kerbing/ re-profiling	Berkeley Avenue	Cul-de-sac section	Request from resident, via Councillor, to re-profile the kerb line to better facilitate access for larger vehicles, that are otherwise mounting and damaging the corner of the verge.	<ul style="list-style-type: none"> • Comment: There would need to be funding available for detailed investigation of the make-up (and buried services, tree roots etc.) that may lay within this area. It would need to be reconstructed to take vehicular traffic, so this work will determine what is necessary (and at what cost) to make this alteration. • Casualty Data: N/A • Anticipated Costs: A detailed investigation would be required before costs can be estimated.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
37	Coley	Restriction Enforcement (Potential)	Lower Field Road	Closure point, near to the junction with Garnet Hill	Residents have reported to Councillor considerable daily contravention of the road closure restriction by motorcyclists, using this as a rat-run between Berkeley Avenue and Castle Hill/Bath Road.	<ul style="list-style-type: none"> • Comment: It is going to be challenging to find an engineering solution that enables the legitimate access, but prevents access for these smaller motorised vehicles. This could be a potential site for future civil enforcement of moving traffic offences, subject to enforcement of this restriction being allowable in the regulations and subject to the vehicles being registered and registration places being displayed. • Casualty Data: No accidents reported in the latest 3 year period (up to August 2021). • Anticipated Costs: Unable to estimate at this time, as it is dependent on a wider piece of work and the types of technology that will be adopted.
38	Coley	Resurfacing, adoption and illuminating footpath	Wensley Road	Links Wensley Road (near North Lodge Mews) with Coley Avenue South (to the south of Froxfield Avenue).	<p>It has been a long-standing desire of the West Reading Area Study to bring this footpath up to adoptable standards, to adopt it as part of the Highway network and to provide street lighting. This will increase the appeal to use it, improving accessibility through the area.</p> <p>The majority of the CIL-funded West Reading Area Study deliverables have been implemented, but there was insufficient funding available to deliver this item.</p>	<ul style="list-style-type: none"> • Comment: This entry was agreed for inclusion as it was a desirable element of the West Reading Study, for which the associated funding was unable to cover. • Casualty Data: 1 serious and 1 slight incident reported in the area in the latest 3 year period (up to August 2021). None involving pedestrians. • Anticipated Costs: Estimated costs (October 2019) £180k total.
39	Coley	Zebra Crossing Upgrade	Wensley Road	Outside shopping area, east of St Saviours Road	There has been a request made, via ward Councillors, for an upgrade of the beacons at the existing zebra crossing to a 'brighter' LED type.	<ul style="list-style-type: none"> • Comment: This is a long standing crossing, but requests have been received to upgrade the type of beacon that is in place to a modern LED type, to further enhance the visibility. • Casualty Data: 1 serious and 1 slight incident reported in the area in the latest 3 year period (up to August 2021). None involving pedestrians. • Anticipated Costs: Estimated costs (December 2019) £5k.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
40	Emmer Green	Zebra Crossing	Caversham Park Road	In place of the uncontrolled crossing between Littlestead Close and the bus stop opposite.	Resident concern about difficulties in crossing the road, particularly for the elderly and for parents with young children. Resident would like a controlled crossing to be installed at this location to improve pedestrian safety.	<ul style="list-style-type: none"> • Comment: Officers have measured the visibility from the crossing, which meets design guidelines. The implementation of a controlled crossing will require movement of the bus stop and hard-standing on the verge and a re-profiling of the footway on the western side. • Casualty Data: No incidents recorded in the latest 3 year period of data (up to August 2021). • Anticipated Costs: A high level estimate would be £80,000.
41	Emmer Green	One way	Grove Road	The section between no 59-87 Grove Rd	Request to make this section one way, due to issues caused by vehicles entering both ends of Grove Road and forcing vehicles to reverse.	<ul style="list-style-type: none"> • Comment: This would require statutory consultation and may receive objections from residents. • Casualty Data: 1 slight accident in the latest 3 year period (up to August 2021) involving a pedestrian crossing the road. • Anticipated Costs: A very high level estimate would be around £20,000.
42	Emmer Green	Speed Calming	Knights Way	Entire Length	Request via MP for speed calming	<ul style="list-style-type: none"> • Comment: Speed surveys should be carried out to assess vehicle speeds. Officers recommend a 20mph scheme with the addition of signs and road markings, as well as traffic calming features such as road humps. • Casualty Data: No injury related accidents reported in the latest 3 year period (August 2021). • Anticipated Costs: A very high level estimate would be around £40,000 for a 20mph scheme with features.
43	Emmer Green	Pedestrian crossings	Lowfield Road	To the east of the Peppard Road junction / entrance to the shops car park, linking to the footpath network.	Requested via MP. Request for controlled pedestrian crossing facility to support walking to/from the shops using the footpath network in the area.	<ul style="list-style-type: none"> • Comment: A detailed investigation would need to be carried out to assess whether or not a crossing can be installed at these locations. • Casualty Data: No incidents involving casualties recorded in the latest 3 year period of data (up to end of September 2022). • Anticipated Costs: It is estimated that a basic zebra crossing installation would cost around £80k, provided that there are no significant engineering challenges (e.g. level/gradient issues, close proximity to suitable electrical supply).

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
44	Emmer Green	Pedestrian crossing and 20mph zone	Lowfield Road	Near the junction with Farnham Drive	Residents have raised concerns about speeding in this area, and have noted that there are now more children crossing the road since the new housing was built on 37-91 Lowfield Road. There have been requests to reduce the speed limit and install a pedestrian crossing.	<ul style="list-style-type: none"> • Comment: It would be beneficial to conduct surveys to assess vehicle speeds and appropriate measures. Speed calming devices could increase noise complaints and will be costly. The area will need to be reviewed to determine the best location for a crossing (considering visibility and desire lines) and the area to be covered by the lower speed limit and traffic calming. A suggestion could be between Galsworthy Drive and Earlsfield Close to cover the bends in the road and the requested crossing location. • Casualty Data: No accidents reported in this area in the latest 3 year period (up to August 2021). • Anticipated Costs: A high level estimate would be around £65,000 for the crossing and minimum of £40,000 for a 20mph zone in a small area with calming.
45	Emmer Green	Pedestrian crossings	Peppard Road	Between Wetherby Close and Caversham Park Road, linking footpaths to/from Clayfield Copse	Requested via MP. Request for controlled pedestrian crossing facility to support walking to/from Clayfield Copse using the footpath network in the area.	<ul style="list-style-type: none"> • Comment: A detailed investigation would need to be carried out to assess whether or not a crossing can be installed at this location. There is a bus stop near Wetherby Close which may need to be relocated, if it will effect the visibility of the crossing. • Casualty Data: No incidents involving casualties recorded in the latest 3 year period of data (up to end of September 2022). • Anticipated Costs: It is estimated that a basic zebra crossing installation would cost around £80k, provided that there are no significant engineering challenges (e.g. level/gradient issues, close proximity to suitable electrical supply).

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
46	Katesgrove	Restriction Enforcement (Potential)	Elgar Road	At the road closure point, between Elgar Road and Elgar Road South.	The road is closed to vehicles and has a TRO in place for this, but allows cyclists and pedestrians through the closure. There is also a footway on either side for pedestrian access. The closure is being abused by users of motorcycles and quadbikes, who are managing to squeeze through the gaps between the bollards, left for legitimate access. Ward Councillors have raised this issue and would like a solution to prevent this.	<ul style="list-style-type: none"> • Comment: It is going to be challenging to find an engineering solution that enables the legitimate access, but prevents access for these smaller motorised vehicles. The bollard gaps are only marginally wider than the minimum guidance given to facilitate cyclist movements. This could be a potential site for future civil enforcement of moving traffic offences, subject to enforcement of this restriction being allowable in the regulations and subject to the vehicles being registered and registration plates being displayed. • Casualty Data: One slight accident at the Elgar Rd/Waterloo Rd junction in the latest 3 year period (up to August 2021) where a driver lost control and hit a bollard. • Anticipated Costs: Unable to estimate at this time, as it is dependant on a wider piece of work and the types of technology that will be adopted.
47	Katesgrove	Weight Restriction	Highgrove Street	Entire road	Request from resident for a weight restriction on this road to restrict HGVs from using this road. Reports of vehicles being damaged on several occasions from large vehicles moving through this area.	<ul style="list-style-type: none"> • Comment: A weight restriction will also restrict some vehicles needing to load/unload on behalf of residents, so could prove to be unpopular overall. • Casualty Data: One slight accident in the latest 3 year period (up to August 2021) not related to HGV traffic. • Anticipated Costs: A very high level estimate would be around £5,000 for the installation of the restriction (the TRO).
48	Katesgrove	20mph	Highgrove Street	Entire road	Complaint about speeding traffic in Highgrove Street by cars using the road as a short cut and because of this a request for a 20mph limit.	<ul style="list-style-type: none"> • Comment: It would be beneficial to conduct surveys to assess vehicle speeds and appropriate measures. Speed calming devices could increase noise complaints and will be costly. • Casualty Data: 1 slight incident reported in the latest 3 year period (up to August 2021) but speeding was not a contributing factor. • Anticipated Costs: A high level estimate would be £40,000.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
49	Katesgrove	Traffic calming /road closure	Home Farm Close	Entire Street affected, closure point to be determined	Councillor request to stop speeding/joy-riding by permanently closing the road, potentially mid-way.	<ul style="list-style-type: none"> • Comment: While this proposal will have a speed-calming impact across the street overall, it still leaves potential on either side, albeit that this would unlikely be as a result of non-resident (and their visitors) traffic. Officers recommend that a 20mph scheme with physical traffic calming measures also be considered. Both options would require statutory consultation for a new TRO. • Casualty Data: No incidents reported in the latest 3 year period (up to August 2021). • Anticipated costs: A high level estimate would be £35,000 for a 20mph scheme. A closure would cost more and would depend on the features installed.
50	Katesgrove	Cycle Facilities	Silver Street & Southampt on Street	Silver Street & Southampton Street	<p>Reallocation of road space to accommodate on-carriageway cycle facilities</p> <p>Minor text update for November 2023: Tranche 1 Active travel schemes agreed as 'permanent' and address the majority of this request, however, section of Southampton Street between Crown Street and Mill Lane/IDR roundabout remains.</p>	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum. Much of this request has been addressed, however, there is a remaining section of Southampton Street between Crown Street and the Mill Lane/IDR roundabout that is untreated. This is being considered as part of major strategic schemes. • Casualty Data: 5 slight and 3 serious incidents reported in the latest 3 year period (up to August 2021). 4 incidents involved cycles and two of these were caused by vehicles entering the bus lane. • Anticipated Costs: Minimal costs to retain existing measures as a 'permanent' scheme.
51	Kentwood	20mph	Armour Hill	Dudley Close Larissa Close area	Requested reduction of speed limit from 30mph to 20mph due to the lack of visibility and perceived speeding in the area. Additional measures could also be investigated to improve visibility of junctions.	<ul style="list-style-type: none"> • Comment: It would be beneficial to conduct surveys to assess vehicle speeds and appropriate measures. The area to which the zone covers needs consideration and could feasibly include the entire street and those no-through roads off of Armour Hill. This would, however, increase the costs of the scheme, with upward of 1km of carriageway to be covered by traffic calming features. • Casualty Data: No incidents involving casualties recorded in the latest 3 year period (up to August 2021). • Anticipated Costs: A high level estimate would be £120,000 for the abovementioned area.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
52	Kentwood	Traffic calming	Kentwood Hill	Request related broadly to the section between Armour Hill and Armour Road.	Concerns about speeding, despite the speed camera, and a request for traffic calming.	<ul style="list-style-type: none"> • Comment: Due to this being a bus route, it is likely that speed cushions would be the highest 'impact' measures that could be introduced. On a 30mph street there would need to be (costly) illuminated warning signs, which also attract ongoing revenue costs. Consideration should be made for making this a 20mph street instead, which would need a range of features and signing within. • Casualty Data: No accidents reported in the latest 3 year period (up to August 2021). • Anticipated Costs: A very high level estimate for a 20mph with traffic calming would be around £50k for this section of Kentwood Hill. This would increase if other roads were included, or if a greater length of the street were to be included.
53	Kentwood	Traffic calming	Oak Tree Road	Whole length	Request received for speed calming measures to address the perception of speeding traffic and rat-running.	<ul style="list-style-type: none"> • Comment: There would need to be consideration about whether there is a desire to lower the speed limit and whether speed survey data and the Police would support this. Traffic calming can be applied to 30mph roads, but will require illuminated signing, which will considerably increase the scheme costs (est. £6k per sign). Officers recommend that a 20 zone with side roads be considered. There is another request on this list for a 20mph scheme on Westwood Rd which could be considered with this one. • Casualty Data: No incidents recorded in the latest 3 year period of data (up to August 2021). • Anticipated Costs: A high level estimate for a 20mph scheme with side roads would be around £100,000.
54	Multiple Caversham Heights / Emmer Green	20mph	St Barnabas Road	Extension of existing scheme, northbound, to Surley Row.	Request received for an extension of the existing 20mph zone in a northbound direction to the junction with Surley Row, including a request for speed calming measures along this section.	<ul style="list-style-type: none"> • Comment: There have been complaints about safety, stating that vehicles get dangerously close to pedestrians especially at school drop off times. It would be beneficial to conduct surveys to assess vehicle speeds and appropriate measures. • Casualty Data: There have been no recorded incidents in the latest 3 year period (up to August 2021). • Anticipated Costs: A high level estimate would be around £35,000 to extend the exiting 20 zone along St Barnabas Rd only (not side streets).

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
55	Multiple: Norcot / Battle	20mph	Residential roads off Oxford Road	Entire streets	In January 2021, Labour Councillors from Battle, Kentwood and Norcot wards conducted a community survey to assess support for the introduction of a new 20mph zone in the area. Of the 219 respondents, more than 80% indicated in favour of introducing this restriction.	<ul style="list-style-type: none"> • Comment: If this proposal is developed, there would need to be supplementary traffic calming features added. It would be possible to implement this large area in phases, but each phase would need to be a compliant, cohesive, standalone zone that could be expanded with further funding at a later date. • Casualty Data: 3 serious and 11 slight accidents reported in the latest 3 year period (up to August 2021). 8 on Water Rd/Grovelands Rd and 2 of these listed speeding as likely causation factors. • Anticipated Costs: A detailed investigation would be required before costs can be estimated for phasing the works, as it would depend on the number of streets and features included in the area. A very high-level estimate of at least £300k+ has been reported to the Sub-Committee for the entire area.
56	Multiple: Park / Redlands	Traffic calming	Eastern Ave	Entire Road	Request for traffic calming such as speed humps in order to reduce vehicle speeds.	<ul style="list-style-type: none"> • Comment: This is in an existing 20mph zone and there are some existing calming features on the south end of the road which could be amended, though that section is used by buses. • Casualty Data: 2 slight and 1 serious incidents reported in the latest 3 year period (up to August 2021). Two were at junctions and the other was on the roundabout. Speeding was not a contributing factor in any of the incidents. • Anticipated Costs: A very high level estimate would be around £55,000. • Recommended Action: Retain
57	Multiple: Caversham / Thames	Walking/ Cycling Improvements	Promenade Road & Caversham Road Roundabout	Promenade Road & Caversham Road Roundabout south of Caversham Bridge	Installation of dropped kerbs to aid access to Abbotsmead Place and Thames Path	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum. • Casualty Data: N/A - this request relates to improved access. • Anticipated Costs: A detailed investigation would be required before costs can be estimated as it would depend on the number of features installed.
58	Multiple: Abbey / Battle / Kentwood	Walking/ Cycling Improvements	Thames Path	Thames Path, Tilehurst to Town Centre	Convert the footpath to shared-use and undertaken improvements as detailed in risk assessment, including surface upgrade, speed reduction measures and signing.	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum. • Casualty Data: N/A - this request relates to increased access. • Anticipated Costs: A detailed investigation would be required before costs can be estimated.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
59	Multiple: Abbey / Caversham / Thames	Cycle Improvements	NCN 5	Caversham	Improve cycle facilities along route 5, or alter route, as part of redevelopment of St Martin's Precinct, including improved signing (i.e. between Abbotsmead Place and Hemdean Road) and additional cycle parking. Diversion of route would need to be agreed with Sustrans.	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum. • Casualty Data: N/A - this request relates to improved access. • Anticipated Costs: A detailed investigation would be required before costs can be estimated.
60	Multiple: Abbey / Katesgrove	Road Marking	Bridge Street	The 'Oracle' roundabout with Southampton Street	Design and implement 'spiral markings' on the roundabout to assist with lane discipline. Reported to March 2014 TMSC.	<ul style="list-style-type: none"> • Comment: A more detailed investigation is needed to ascertain feasibility due to the traffic lights. Potential alterations to yellow-box junctions, as part of forthcoming civil enforcement of moving traffic offences, may be necessary and these may be complimentary works funded by capital investment. • Casualty Data: 6 slight incidents reported on the roundabout in the latest 3 year period (up to August 2021) however, 4 of these incidents can be attributed to lane-changing. • Anticipated Costs: A high level estimate would be around £30,000 due to the level of traffic management required.
61	Multiple: Abbey / Thames	Walking/ Cycling Improvements	Caversham Road	South of Northfield Road	Cyclists are unable to turn right out of Northfield Road towards town - they have to navigate Caversham Road roundabout. Upgrade existing pedestrian crossings on Caversham Road (by Northfield Road) to toucan crossings.	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum. This upgrade is hoped to be delivered, or at least funded, by development in this area but will remain on this list until this is confirmed. • Casualty Data: 7 slight and 1 serious accidents reported on the Caversham Rd roundabout in the latest 3 year period (up to August 2021). 4 of these incidents involved pedal cycles. • Anticipated Costs: A detailed investigation would be required before costs can be estimated, but it is hoped that this will be funded/delivered by development works in the vicinity.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
62	Multiple: Abbey / Thames	Cycle Facilities	Watlington Street & Forbury Road	Watlington Street & Forbury Road, providing linking to Christchurch Bridge via Kings Meadow	Reallocate road space to pedestrians and cyclists through provision of segregated facilities, potentially kerb segregated. This would link Reading Station with NCN 422, and the new development site near Kenavon Drive. A high quality, strategic cycle route could be developed here. Induction loops at toucan crossings along Forbury Road and Watlington Street could be installed if not already in place.	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum. • Casualty Data: N/A • Anticipated Costs: A detailed investigation would be required before costs can be estimated as it would depend on what measures can be installed.
63	Multiple: Abbey / Thames	Junction improvement (pedestrians)	Watlington Street/Kings Road	Crossings at the meeting of Watlington Street/Forbury Road and Kings Road	Area Neighbourhood Officer has raised concerns regarding the inconsistency of tactile paving at the sites of the older traffic signal controlled pedestrian crossings.	<ul style="list-style-type: none"> • Comment: This work will likely require footway improvement works around the junction, in addition to the installation of tactile paving. This may be covered by future capital investment bids. • Casualty Data: 2 slight and 1 serious incident reported in this area in the latest 3 year period (up to August 2021). None involved pedestrians. • Anticipated Costs: A detailed investigation would be required before costs can be estimated as it would depend on the extent of the work needed.
64	Multiple: Abbey, Battle, Norcot, Kentwood	Cycle Access	Oxford Road	Entire Road	Request for improved cycle facilities along the Oxford Road corridor	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum and an action to capture requests made by the forum that are not already on the main list of requests. It is expected that the Oxford Road Corridor Study will incorporate some improvements. Elements such as the Red Route and Active Travel Tranche 1 (if agreed to be made permanent) were intended to partially address this request. • Casualty Data: N/A - relates to improved access. • Anticipated Costs: To be confirmed, but will form part of a wider scope of works.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
65	Multiple: Caversham / Caversham Heights	Speed calming	Kidmore Road	Section between Highmoor Road and The Mount	<p>A petition from residents of Kidmore Road was presented to the Sub-Committee in September 2022. The petition highlighted their concerns regarding the trees, rat running, speeding, road safety and the state of the pavement on Kidmore Road. A separate letter sent by MP Matt Rodda suggested that residents were in favour of a 20mph speed limit as well as other traffic calming features such as humps, chicanes and vehicle activated signs.</p> <p>A meeting between Ward Councillors, officers and representatives of residents took place in November 2022. Issues and potential mitigations were discussed and it was broadly agreed that 20mph with speed calming was desirable and whether some speed calming features could create build-outs around the most 'problematic' tree locations.</p>	<ul style="list-style-type: none"> • Comment: 20mph for this relatively narrow section of road seems entirely appropriate, but will require a robust set of physical features for compliance (making the restriction 'self-enforcing'). This comes with compromises, such as potential increases in road noise, which were discussed in the resident meeting. Options for creating more walkway space around the trees were discussed and is going to be challenging, so a proposed solution is not yet clear. One option discussed was creating some build-outs as part of the speed calming scheme, but proximity of driveways, drainage and the inevitability of queuing traffic are significant factors to consider. Speed calming and 20mph may create a beneficial reduction in the reported rat-running and overall traffic volumes. • Casualty Data: No incidents involving casualties recorded in the latest 3 year period of data (up to end of September 2022). • Anticipated Costs: A high-level estimate for delivering the core elements of a 20mph zone (TRO, signing, lining and an assumption of full-width speed humps) is £100k.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
66	Multiple: Caversham / Thames	Footway and Junction improvements (vehicles & pedestrians)	Gosbrook Road	Jcn Westfield Road	Resident has reported the issue with long vehicles turning left onto Westfield Road causing damage to wall of No.4, due to poor driving. Resident has asked for alteration to island or no-left-turn etc. to prevent this occurring. General concerns have been raised regarding the narrow footway width along Gosbrook Road.	<ul style="list-style-type: none"> • Comment: The size of the island was reduced when the traffic signals were removed from this junction. It reinforces the no-right-turn onto Gosbrook Road and houses illuminated signs. It also acts as an informal refuge island for pedestrians. These factors need to be taken into account if any alterations are being considered. Footway widening may be technically possible and will be of widespread benefit to pedestrians, but will be costly. • Casualty Data: 3 slight incidents reported near the junction in the latest 3 year period (up to August 2021). 1 vehicle failed to give way, one was distracted and one failed to indicate left. • Anticipated Costs: A detailed investigation would be required before costs can be estimated as it would depend on what features can be installed and what underground services may be impacted by the necessary civil engineering works.
67	Multiple: Caversham / Thames	Pedestrian Crossing	Gosbrook Road	Between George Street and Briants Avenue	Request, via Councillor, to consider a crossing facility along this stretch of road.	<ul style="list-style-type: none"> • Comment: Investigation would be required to ascertain desire-lines (popular 'destinations') and feasibility (junctions, dropped kerbs, parking etc.). The type of facility (informal or controlled) can then be considered. • Casualty Data: 2 slight incidents in the latest 3 year period (up to August 2021). None involving pedestrians. • Anticipated Costs: A crossing could cost around £60k-80k depending on the location and the level of works required.
68	Multiple: Caversham / Thames	Vehicle restriction	School Lane	Entire road	Request to prevent vehicles from using School Lane by installing bollards at each end. It is a single lane road with no pavements and is used regularly by cyclists and pedestrians. Concerns that the development of the New Directions site could increase the number of vehicles using this lane and risk pedestrian and cyclist safety.	<ul style="list-style-type: none"> • Comment: Bollards would prevent cars entering the road, however, the features need to remain accessible for mobility aids and pushchairs etc., so could still be open to potential abuse by smaller motorised vehicles. We also need to ensure that any proposal doesn't create additional obstacles for those with impaired vision. This will likely require a TRO motor vehicle prohibition to be consulted and implemented. • Casualty Data: No accidents reported in the latest 3 year period (up to August 2021). • Anticipated Costs: A very high level estimate would be £6k.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
69	Multiple: Caversham / Thames	20mph	Various	Lower Caversham	<p>A report to Sept 2016 TMSC proposed a 20mph zone that could cover the Lower Caversham and Amersham Road estate areas. This report was the result of a number of petitions and requests for 20mph in these areas. It was agreed that there would need to be further consultation with Councillors and CADRA, but noted that there was currently no funding for the scheme.</p> <p>At the request of Caversham Ward Councillors, this amendment for November 2022 separates out the Amersham Road area from the remainder of the proposal.</p>	<ul style="list-style-type: none"> • Comment: This scheme is awaiting funding to enable it to be fully investigated (e.g. conducting speed surveys) and to progress to detailed design and implementation. The outline area in the original report is very large, but this could be split into prioritised phases and it has been requested to separate the Amersham Road area from this wider area for reporting. • Casualty Data: This will be investigated, alongside surveys, as the scope of the scheme is developed. • Anticipated Costs: A detailed investigation would be required before costs can be estimated as it would depend on the extent of the scheme.
70 Page 74	Multiple: Caversham Heights / Caversham	Pedestrian Crossing	Oakley Road	Close to junction with Hemdean/Rother field	Concerns have been raised to Ward Councillor and officers about the number of pedestrians that cross on the Rotherfield Way and Oakley Road sides of this roundabout and controlled facilities have been requested.	<ul style="list-style-type: none"> • Comment: For safety, controlled crossings require good (and specified) advance visibility and to be away from junctions. Either side will be challenging, as there are dropped kerbs for driveway accesses, junctions nearby and bus stops that would need to be relocated. While detailed investigation would be required, it may be the case that a controlled crossing is not achievable near to the desire lines but that some other enhancements/informal features may help. • Casualty Data: No incidents recorded in the latest 3 year period of data (up to August 2021). • Anticipated Costs: A high level estimate for a 'standard' zebra crossing at this location would be £80,000, factoring in the strong likelihood that kerbing works, parking restrictions and bus stop/infrastructure movement would be required.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
71	Multiple: Church / Redlands	20mph	Shinfield Road / Christchurch Road	Entire length	<p>Request made by the Cycle Forum at their meeting in November 2021 for a scheme that introduces 20mph, to compliment the active travel scheme.</p> <p>Officers have included Christchurch Road, reflecting the officer comment on this item.</p>	<ul style="list-style-type: none"> • Comment: Officers consider that there could be beneficial and appropriate application of 20mph restrictions at certain locations on the road, particularly around the parade of shops on Christchurch Road and enhancements around the Shinfield Rise shops. This is due to the increased footfall expected at these 'destination' locations. However, it is not necessarily considered appropriate for the entire length of Shinfield Road. • Casualty Data: 8 slight and 3 serious incidents reported along all of Shinfield Road in the latest 3 year period (up to end May 2022). 5 incidents involved pedal cycles and 1 incident noted speeding as a contributing factor. • Anticipated Costs: Depending on the measures installed, an estimate for enhancing traffic calming features around the Shinfield Rise shopping area and adding a small zone around the Christchurch Road shops is £80,000. A zone covering the entirety of Shinfield Road is expected to exceed £150,000.
72	Multiple: Church / Whitley	20mph	Hartland Road & Whitley Wood Road	From Basingstoke Road to Shinfield Road	Request, via Councillor, for 20mph speed reduction to improve the environment for residents, reduce the appeal as a cut through and to reduce safety risks in consideration of the nearby schools	<ul style="list-style-type: none"> • Comment: A speed survey will be necessary to consider suitability and in supporting the consultation with the Police. Officers recommend a 20mph zone for this type of residential street as it would add a range of supporting physical measures to improve compliance. These will need careful consideration in the context of the types of vehicles using the street (e.g. buses) and around drainage/ponding risks along the Whitley Wood Road hill. • Casualty Data: 5 slight accidents on Hartland Road and 3 slight accidents reported on Whitley Wood Rd in the latest 3 year period (up to August 2021). No accidents where speeding was considered a contributing factor. Most of the accidents relate to junction collisions. • Anticipated Costs: A high level estimate would be £200,000 for Hartland Rd and Whitley Wood Road though side roads should also be considered and would increase the costs further.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
73	Multiple: Norcot / Southcote	20mph zone	Shilling Close / Honey End Lane	Whole of Shilling Close and section of Honey End Lane between The Meadway and Shilling Close	Request for a 20mph zone due to concerns regarding safety, due to vehicle speeds. This was raised alongside concerns about parking (including footway parking) on Shilling Close and a request to place restrictions throughout - parking likely to be contributing to the risks and will be considered in the Waiting Restriction Review Programme.	<ul style="list-style-type: none"> • Comment: Due to the severance of Honey End Lane at Shilling Close, this would be an appropriate and cohesive scheme. Physical traffic calming features (speed humps) would be required to make the scheme compliant, which may generate a level of local objection - they are indiscriminate features affecting all. It should be noted that this would apply only to areas of adopted Highway - there are sections of unadopted carriageway within the close. • Casualty Data: No incidents involving casualties recorded within the latest 3 years of data (up to end May 2022). • Anticipated Costs: A very high level estimate would be around £70,000
74	Multiple: Norcot / Tilehurst	Pedestrian crossings	Usk Road	Near the school	Request for a zebra crossing near the school due to concerns about the safety of school children.	<ul style="list-style-type: none"> • Comment: A detailed investigation would need to be carried out to determine the feasibility of a crossing at this location, as it is not likely that a crossing can be installed anywhere else in this area. • Casualty Data: No incidents reported in the latest 3 year period (up to end May 2022). • Anticipated Costs: If a controlled crossing can be installed, a very high level estimate would be around £80,000, but could be considerably higher depending on any special engineering requirements. Detailed investigation is required.
75	Multiple: Park / Redlands	Pedestrian crossing enhancements	Whiteknights Road	Roundabout with Upper Redlands Road	Concern has been raised with Councillor regarding pedestrians crossing the road from the University campus. Request made for enhancements at this difficult location.	<ul style="list-style-type: none"> • Comment: Officers have initially suggested consideration of pedestrian refuge islands (subject to feasibility) at the roundabout exits. These would slow traffic by removing opportunities to cut across hatched areas and allow pedestrians to cross in two parts. Potential re-profiling of the campus exit could also encourage pedestrians to cross further back from the roundabout to improve visibility. These will be relatively costly civils works, for which there would also need to be some vehicle tracking conducted, to ensure that longer vehicles could safely navigate a 'tightened' roundabout. Unfortunately, the exit and desire line are currently too close to the roundabout to place a controlled crossing facility. • Casualty Data: 1 slight incident recorded in the latest 3 year period of data (up to August 2021). This incident did involve pedestrians. • Anticipated Costs: A detailed investigation would be required before costs can be estimated.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
76	Multiple: Tilehurst / Kentwood	20mph	Westwood Road	Whole length	Request received for a reduced speed limit and traffic calming measures to be installed.	<ul style="list-style-type: none"> • Comment: If this proposal is developed, there would need to be supplementary traffic calming features added. There would need to be careful consideration of the type of measure, as this is a bus route and will be a key emergency service vehicle route for parts of Tilehurst and beyond. Side roads should also be considered for inclusion. There is a separate request on this list for traffic calming on Oak Tree Road which could be considered with this one and would slightly reduce the overall combined costs, versus implementing them separately. • Casualty Data: 1 slight incident at the School Road junction reported in the latest 3 year period (up to August 2021) but it did not list speeding as a causation factor or involve pedestrians. • Anticipated Costs: A high level estimate would be £100,000 but this would increase if additional roads are included in the zone.
77 Page 77	Multiple: Tilehurst / Norcot	20mph	Elvaston Way & wider Tilehurst area	From Stanham Road to Taff Way	Raised by ward Councillor.	<ul style="list-style-type: none"> • Comment: Dee Road is already included in a 20mph zone but we could expand the zone to include Stanham Rd, Combe Rd, Elvaston Way, Tern Close and Taff Way. It would be beneficial to conduct surveys to assess vehicle speeds and appropriate measures. There is some traffic calming in the area, but some illuminated signage (not required for 20mph) would need to be removing and there remains quite a significant overall length of carriageway that would require treatment. • Casualty Data: 2 slight incidents reported in the area in the latest 3 year period (up to August 2021). None where speeding was considered a contributing factor. • Anticipated Costs: Survey: A high level estimate would be around £120,000 for the roads listed here.
78	Multiple: Various	Walking/ Cycling Improvements	Various	Portman Road Palmer Park Caversham Bridge Richfield Avenue	Improved clarity of shared-use facilities. For example: installation of tiles	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum. • Casualty Data: N/A • Anticipated Costs: A detailed investigation would be required before costs can be estimated.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
79	Norcot	20mph zone	Cockney Hill	Close to Prospect School	Request to install a 20mph zone around the school due to concerns about the safety of school children in the area.	<ul style="list-style-type: none"> • Comment: There are existing traffic calming features in the area so a 20mph zone could be installed at a relatively low cost with signs and road markings. Consideration should be made as to whether or not other roads in the area should be included in the zone. • Casualty Data: No incidents reported in the latest 3 year period (up to end May 2022). • Anticipated Costs: Costs can be estimated once it is decided how many roads should be included in the scheme. The estimate for Cockney Hill alone would be around £10,000 if no additional humps are installed, but there may be a compliance requirement for additional features.
80	Norcot	Road Closure	Craig Ave	At its junction with Grovelands Rd	Complaint from a resident stating that vehicles exiting Craig Ave cause unnecessary delays when they head eastbound down the Oxford Road.	<ul style="list-style-type: none"> • Comment: While officers understand the concerns raised, we are not aware of significant demand for this change. If the proposal is developed, it would require statutory consultation, which would provide opportunity for objection (and support), but an initial, simple informal consultation may be beneficial (and cost-effective) in the first instance. • Casualty Data: No incidents recorded in the latest 3 year period of data (up to August 2021). • Anticipated Costs: A detailed investigation would be required before costs can be estimated.
81	Park	Remove/ reduce rat-run	Crescent Road	Particularly between Wokingham Road and Bulmershe Road	<p>Concerns have been raised about the volume of traffic that can rat-run across east Reading using Crescent Road. Discussions have taken place at TMSC and with the East Reading Area Study Steering Group, but an agreeable solution is yet to be found.</p> <p>Proposed solutions have included reviewing streets to the east of Wokingham Road, which can also facilitate this cut-through movement.</p>	<ul style="list-style-type: none"> • Comment: An agreeable solution needs to be found and funded. It will not be possible to cost or fully analyse the potential benefits/impact at this stage, but the outcome would likely be a restriction (e.g. directional) that could impact on local accessibility to the area and could be controversial when consulted. • Casualty Data: No incidents recorded in the latest 3 year period of data (up to August 2021). • Anticipated Costs: A detailed investigation would be required before costs can be estimated.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
82	Park	Construct new footway	Hamilton Road	Southern end of the road, leading from Whiteknights Road to the school entrance on the eastern side.	Request from ward Councillor for the construction of a footway - there is currently no footway on the eastern side of the street, leading up to the school entrance.	<ul style="list-style-type: none"> • Comment: Significant feasibility issues. There is currently insufficient Highway land to install the footway, which would require agreement to move the highway boundary into private land. This section of land would then need to be cleared back, which includes fencing, trees and other vegetation. Construction of the footway would also necessitate Highway drainage installation, movement of street lighting and potential utility diversion. • Casualty Data: No accidents reported in this area in the latest 3 year period (up to August 2021). • Anticipated Costs: Unable to estimate at this time due to the aspects around feasibility and likely requirement for land purchase (subject to agreement).
83	Park	Road Closure	Heath Road	One end	Councillor request to close off one end of Heath Road to prevent speeding and rat running	<ul style="list-style-type: none"> • Comment: This would require statutory consultation and may receive objections from residents, who may have significant diversions to reach their destination, or to find alternative parking. There will need to be a reduction in on-street parking availability to facilitate turning areas. There will not necessarily be a reduction in speeds, but this would prevent rat-running, which would then likely be pushed to neighbouring streets - this may also generate objections. • Casualty Data: No incidents recorded in the latest 3 year period of data (up to August 2021). • Anticipated Costs: A detailed investigation would be required for the preferred solution before costs can be estimated.
84	Park	One way plug	Holmes Road	One end	Councillor request to use a plug to make Holmes Road one way following petition from residents.	<ul style="list-style-type: none"> • Comment: Feasibility concerns with regards to the access challenges that this will create for residents and the rear of the fire station and displacement of traffic onto neighbouring roads (Early Hill Road is a private road, for example), which will likely generate objections. The proposal would require statutory consultation and it is likely that some reduction of on-street parking will be required to accommodate the plug, which will be set back to facilitate turning in the junction (a further feasibility concern). It should also be noted that the no-entry restriction will be Police-enforceable only for the foreseeable future. • Casualty Data: 3 slight accidents at the junction with Wokingham Road in the latest 3 year period (up to August 2021). • Anticipated Costs: A high level estimate would be £65,000 for a feature, which would not include any decorative items such as a planter.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
85	Park	No right turn	Liverpool Road	Approaching the junction with London Road	Councillor request to ban the right-turn onto London Road to reduce waiting times for traffic approaching the junction. Proposed that motorists wishing to turn right travel to the roundabout with the A3290 to come back into Reading.	<ul style="list-style-type: none"> • Comment: A survey could be conducted to ascertain how many vehicles are turning right from this junction. • Casualty Data: 2 slight incidents reported at the junction with London Road in the latest 3 year period (up to August 2021). • Anticipated Costs: A high level estimate would be £10,000 depending on sign requirements.
86	Park	Weight restriction	Palmer Park Avenue	Culver Lane Bridge, and the streets linking from Wokingham Road/Church Road to the bridge.	Request from ward Councillor to prevent HGVs from using St Peter's Road, Brighton Road, Wykeham Road and Palmer Park Avenue to gain access to Wokingham via Culver Lane bridge.	<ul style="list-style-type: none"> • Comment: It should first be noted that such an area restriction would need to extend outside of the Reading Borough Council boundary, so would require collaboration with and agreement from Wokingham Borough Council - the entirety of the Culver Lane bridges is within their local authority area. This would also be the case for restrictions wholly within Reading Borough Council, as the implications will impact on their Highway network. There is an existing 3.6m height restriction in place on the bridge which will deter some larger vehicles from using this route. It should also be noted that we must continue to allow access to the area for large vehicles in order to carry out deliveries/house moving/refuse collection etc for residents of the area and as such, this would be a challenging restriction to enforce for both the police and the Council. It may not deter the most persistent offenders using the route as a cut through to Wokingham. • Casualty Data: No injury related accidents involving HGVs have been reported in this area in the latest 3 year period of data (up to end of September 2022). • Anticipated Costs: This will be dependant on the full extent of the scheme. In the immediate locality this would require a TRO and regulatory (illuminated) signing. More widely, will be advance warning signs and a likely strategic HGV diversion route signed within both local authority areas. Each illuminated regulatory sign is estimated to cost ~£7-8k.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
87	Park	Traffic calming	St Bartholomews Road	Entire road	<p>Councillor request to introduce traffic calming to St Bartholomews Road which is in a 20 zone.</p> <p>Amendment (for March 2023): A petition was reported to TMS in January 2023, containing 64 signatures. This petitioned the Council to tackle speeding on this street.</p>	<ul style="list-style-type: none"> • Comment: Depending on the measure(s), there may need to be some loss of parking. The features will likely necessitate statutory consultation. • Casualty Data: No incidents recorded in the latest 3 year period of data (up to end of September 2022). • Anticipated Costs: A high level cost estimate is £45,000 on the basis of consulting and implementing a range of speed humps/cushions along the street.
88	Park	Pedestrian crossing	St Bartholomews Road	At the junction of St Bartholomews with London Road going east/west along London Road	Councillor request to introduce a pedestrian crossing.	<ul style="list-style-type: none"> • Comment: To be on the likely desire line for pedestrians, this would need to be incorporated into the signalised junction. This will require upgrades, additions and reconfiguring of the junction and to the regional traffic flow management system (SCOOT) by specialist contractors. • Casualty Data: No incidents recorded in the latest 3 year period of data (up to August 2021). • Anticipated Costs: A detailed investigation would be required before costs can be estimated.
89	Park	20mph enhancements	St Peters Road	Entire Length	Request via Ward Councillor for additions to the existing physical traffic calming features and/or potentially raising the height of existing speed humps to address concerns about speeding.	<ul style="list-style-type: none"> • Comment: St Peters Road complies with the requirements of a 20mph zone and has a number of existing full length road humps. It would be useful to conduct speed surveys to assess vehicle speeds and then determine if the existing humps should be upgraded. • Casualty Data: No injury related accidents reported in the latest 3 year period (August 2021). • Anticipated Costs: A very high level estimate would be around £45,000.
90	Park	Crossing improvement	Wykeham Road	At junction with Brighton Road	Improvements needed to allow prams and wheelchair users to cross safely. There have been reports that some users have had difficulties and become stuck when crossing at this junction.	<ul style="list-style-type: none"> • Comment: There are some feasibility issues, with a few driveways at this location which could prevent any crossing points from being installed. There is also a high demand for on street parking in the area which should also be considered before removing any parking spaces. • Casualty Data: No accidents reported in this area in the latest 3 year period (up to August 2021). • Anticipated Costs: A high level estimate would be around £15,000 to make some improvements.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
91	Redlands	Pedestrian crossings	Craven Road	Junction with London Road	Request made by Councillor for the addition of a pedestrian phase to the existing signalised junction - this approach is currently uncontrolled for pedestrians. This has been raised by parents in the context of walking to/from school.	<ul style="list-style-type: none"> • Comment: This will require significant traffic signal alteration works and potential complete technical upgrade of the junction, in addition to reconfiguration of regional control software. It should also be noted that it will provide another opportunity for a junction 'all-red' to be triggered, which will impact on vehicular traffic flow during busier times. It is acknowledged that this alteration will bring greater confidence to those using the junction and could lead could encourage more walking for school travel. • Casualty Data: No incidents involving casualties recorded within the latest 3 years of data (up to end May 2022) involving pedestrians. • Anticipated Costs: Unknown at this time. Specialists would need to be commissioned to review the junction, ducting condition and provide anticipated upgrade costs.
92	Redlands	Pedestrian crossings	Craven Road	Near no.19	Request to upgrade the existing informal crossing outside the nursery at no.19 to a zebra crossing.	<ul style="list-style-type: none"> • Comment: A detailed investigation will be required to ensure that a crossing can be installed at this location, including a full road safety audit. • Casualty Data: No incidents involving casualties recorded within the latest 3 years of data (up to end May 2022) involving pedestrians. • Anticipated Costs: A very high level estimate would be around £90,000, if a zebra crossing could be installed, taking into account island alterations and electrical works.
93	Redlands	Speed calming features	Eldon Terrace	Entire street and immediate area	Request, via Councillor, for the installation of physical speed calming measures to aid motorist compliance.	<ul style="list-style-type: none"> • Comment: The street, and those leading to it, sit within an existing 20mph zone, which will negate the need for additional signing to be implemented alongside any vertical traffic calming measures. Considering the narrow nature of the streets and a level of on-street parking, chicanes or width restricting features are unlikely to be feasible. To improve compliance with the speed limit, speed humps will be the most effective measure. These features, however, will affect all motorists and there is often local concern of noise and vibration raised when such features are proposed to be installed in residential areas. Such features will require public consultation. • Casualty Data: No accidents reported in this area in the latest 3 year period (up to August 2021). • Anticipated Costs: A high level estimate for traffic calming in the entire area would be around £40,000.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
94	Redlands	Traffic calming / One Way	Elmhurst Road, Marlborough Avenue and Redlands Road	Entire Road	Request from residents for traffic calming features such as speed humps to reduce vehicle speeds on these roads. Updated to include Marlborough Avenue, following presentation of the petition at September 2021 TMS. A further request has since been made to also consider Redlands Road and a possible one way system.	<ul style="list-style-type: none"> • Comment: The installation of traffic calming could result in noise complaints and will be costly. It may be beneficial to conduct a speed survey to assess vehicle speeds and investigation is needed to determine what measures could be appropriate here. Making Redlands Rd one-way will have an impact on the hospital and bus services as well as residential roads in the area so will need a more detailed investigation before its feasibility can be determined. • Casualty Data: 3 slight and 1 serious accidents around the Elmhurst Rd/Upper Redlands Rd junction and 2 serious and 4 slight accidents reported on Redlands Rd in the latest 3 year period (up to August 2021). 1 in 2019 on Redlands Rd where speeding was considered a contributing factor. • Anticipated Costs: Very high level estimates would be around £30,000 for traffic calming in Elmhurst Road and Marlborough Ave, depending on the features. One way on Redlands Road would need further investigation before costs can be determined.
95	Redlands	Traffic calming	Erleigh road	Entire length	Request to increase the height of the existing traffic calming measures on Erleigh Road, and to install additional ones where possible. This is due to concerns that motorists can speed over the existing humps and the area is busy with pedestrians and school children.	<ul style="list-style-type: none"> • Comment: Speed surveys should be carried out to assess vehicle speeds to determine if the entire road could benefit from additional calming measures. There is scope to alter existing and to install additional measures such as humps and repeater signs to improve speed compliance, although it should be noted that these will likely not eradicate the issues raised for those who are already wilfully driving inconsiderately. • Casualty Data: 1 serious and 2 slight incidents reported in the latest 3 year period of data (up to end May 2022). No pedestrians were involved and speeding was not considered a contributing factor in any of the incidents. • Anticipated Costs: A very high level estimate would be £55,000 but could increase significantly depending on the number of traffic calming features installed.
96	Redlands	Road Closure	Lydford Road	Between its junctions with Alexandra Road and Donnington Gardens	Request to install bollards to prevent traffic from going through Donnington Gardens to get to Lydford Road - there have been complaints about people accessing the school to pick/up drop off here and there is a perceived speeding issue.	<ul style="list-style-type: none"> • Comment: This will require statutory consultation and the resultant solution would need to cater for legitimate access to the area (e.g. emergency service, property access, utility service providers). • Casualty Data: No incidents recorded in the latest 3 year period of data (up to August 2021). • Anticipated Costs: A detailed investigation would be required before costs can be estimated. A high-level estimate would be £8,000.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
97	Southcote	Walking/ Cycling Improvements	Southcote Farm Lane	Southcote Farm Lane & off-carriageway links to Southcote Primary School	Improve surface of Southcote Farm Lane and convert routes linking to Southcote Primary School to shared-use	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum. • Casualty Data: N/A - this request relates to improved access. • Anticipated Costs: A detailed investigation would be required before costs can be estimated.
98	Thames	Pedestrian Crossing	George Street (B3345)	North of the roundabout with Vastern Road and Napier Road	Businesses have requested the installation of an assisted pedestrian crossing to the north of this roundabout. A report to June 2017 TMS referred to this request and an indicated funding contribution by the business community.	<ul style="list-style-type: none"> • Comment: Project will need to consider feasibility of implementing a crossing (bridge structure, forward visibility), traffic impact when considering options. The crossing would need to be set back from the roundabout from a forward visibility perspective, which moves it onto the bridge structure and away from the crossing desire line. Visibility along the bridge is also a concern due to the pronounced 'hump' mid way. • Casualty Data: 4 serious and 9 slight incidents reported on the roundabout in the latest 3 year period (up to August 2021). None involved pedestrians crossing. • Anticipated Costs: If a controlled crossing can be installed, a very high level estimate would be around £80,000, but could be considerably higher depending on any special engineering requirements. Detailed investigation is required.
99	Thames	Pedestrian crossing	Gosbrook Road	Near its junction with St Johns Road.	<p>A resident has asked for a pedestrian crossing near the junction with St Johns Road as it is used by many pedestrians and visibility towards Briants Ave is poor. The nearby traffic island is not very convenient and a new crossing at this location would be welcomed.</p> <p>Amendment (for March 2023): A further request has been received for a pedestrian crossing facility, such as a refuge island near to the bus stop, which is closer to the junction with George Street. Concerns raised about distances to nearest crossings and the speed of motorists through this section, in addition to some visibility issues caused by parked vehicles.</p>	<ul style="list-style-type: none"> • Comment: There are some significant feasibility concerns at both locations due to the number of accesses to off-street parking places that would create hazards for potential controlled crossing locations and would be obstructed (and create hazards) should refuge islands be considered. The proposals would require restricting parking to obtain the intervisibility for the facility. • Casualty Data: No incidents reported in the last 3 years (up to end September 2022). • Anticipated Costs: If a controlled crossing is feasible, a very high level estimate would be around £80,000 each, but could be considerably higher depending on any special engineering requirements. Pedestrian refuge islands, if feasible, would be estimated at £10,000 each, if feasible. Detailed investigation is required. <p>Recommended action: Retain</p>

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
100	Thames	Speed calming	Napier Road	Entire road	Requests from residents for speed calming due to concerns about vehicles speeding when going to the nearby superstore. Residents say that vehicles do not slow down when approaching the existing zebra crossing and there are concerns about safety due to the increased number of pedestrians using this road.	<ul style="list-style-type: none"> • Comment: It would be beneficial to conduct surveys to assess vehicle speeds and appropriate measures. Speed calming devices could increase noise complaints and will be costly. Illuminated signs are also costly in a 30mph road, with ongoing revenue implications. Due to the bus and delivery traffic along the road, speed cushions are likely to be the most 'impactive' measures that could be introduced. • Casualty Data: 1 slight incident reported in the latest 3 year period (up to August 2021) where a pedestrian was involved but speeding was not considered a contributing factor. • Anticipated Costs: A high level estimate would be £50,000.
101	Thames	Cycle Access	Vastern Road	Right turn into Trooper Potts Way	TRO amendment to enable right-turn from Vastern Road bus lane into Trooper Potts Way	<ul style="list-style-type: none"> • Comment: This has arisen from the Cycle Forum. This would necessitate TRO alteration (consultation), signing changes and very likely require some traffic signal detection alterations to ensure that bicycles would be detected at the junction. • Casualty Data: N/A - this request relates to increased access. • Anticipated Costs: A high level estimate would be around £10,000.
102	Tilehurst	Pedestrian Crossing / Traffic Calming	Chapel Hill	Near to junction with Normanstead Road and also between its junctions with Westwood Glen and Clements Mead	Request for a pedestrian crossing facility to assist with walking to/from Birch Copse primary school in the vicinity of Normanstead Road, with traffic calming measures. A separate request has also been received for a crossing between its junctions with Westwood Glen and Clements Mead due to concerns about pedestrian safety.	<ul style="list-style-type: none"> • Comment: An uncontrolled crossing will be significantly less costly, compared with a controlled crossing (e.g. zebra or traffic signals), as it will not require electrical connections. Options such as a raised table could be considered - this could compliment the separate request for traffic calming along the street. Officers recommend that additional features near Westwood Glen also be considered as part of this request. It is not likely that a controlled crossing can be installed there, but Officers could investigate this further to establish if an island or dropped kerb could be installed to help pedestrians cross in this area as well. • Casualty Data: No incidents recorded in the latest 3 year period of data (up to end May 2022). • Anticipated Costs: A high level estimate for a zebra crossing here would be £80,000. Measures such as humps could increase the cost significantly in addition, or could form an informal facility on their own at a lower cost.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
103	Tilehurst	Pedestrian crossing	Church End Lane	Close to the junction with Norcot Road.	Request for a crossing at this junction due to concerns about pedestrian safety. The junction is busy and there is also a school nearby.	<ul style="list-style-type: none"> • Comment: Due to the number of off street parking places and the proximity of the Chichester Road junction, it is not likely that a zebra crossing can be installed at the desire line. Installing a crossing further south may result in it not being used. Officers will need to investigate this further to establish what measures could be installed to help pedestrians cross this junction. A traffic island may be possible, for example. • Casualty Data: No incidents reported in the latest 3 year period (up to end May 2022). • Anticipated Costs: Costs can be estimated once a detailed investigation is made to determine what features could be installed here.
104	Tilehurst	Road closure	Gratwicke Road	Junction with Corwen Road	Request received for the closure of the road at the junction with Corwen Road to prevent the alleged rat-running of traffic trying to bypass the Norcot Road/Armour Road/Kentwood Hill/School Road junction.	<ul style="list-style-type: none"> • Comment: The proposal would limit access to the street, by severing access via Tilehurst Road. This request raises similar issues to that for Recreation Road. It would be advisable that an informal consultation be conducted with residents prior to developing any proposals, should it appear that funding is likely to be forthcoming. • Casualty Data: No incidents recorded in the latest 3 year period of data (up to August 2021). • Anticipated Costs: A detailed investigation would be required before costs can be estimated.
105	Tilehurst	Pedestrian crossings	Park Lane	Near City Road	Via MP and ward Councillor. Request for crossing, or even refuge island, to support children from Burlington Road attending Little Heath School.	<ul style="list-style-type: none"> • Comment: A detailed investigation would need to be carried out to determine the feasibility of a crossing or refuge island at this location. It may not be possible to install these features due to the proximity of bus stops (visibility), numerous driveway accesses (vehicle movements), speed camera monitoring area impact and road width (for island). • Casualty Data: 1 slight incident involving a pedestrian reported in the latest 3 year period (up to end May 2022). • Anticipated Costs: If a controlled crossing can be installed, a very high level estimate would be around £80,000, but could be considerably higher depending on any special engineering requirements. Detailed investigation is required.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
106	Tilehurst	20mph zone, One-way plug and pedestrian crossing	Recreation Road	Entire length, considering Blundells Road also.	<p>A petition to September 2014 TMSO requested measures to address rat-running traffic and perceived traffic speeding issues. The petition included a request for 20mph speed limits and consideration of a one-way plug.</p> <p>In September 2021 officers received additional request for 20mph and for a pedestrian crossing outside the park.</p>	<ul style="list-style-type: none"> • Comment: It would be beneficial to conduct speed and traffic flow surveys (the traffic flow surveys should be conducted during - and outside of - school holidays) to provide the data for consideration in any proposals. There are feasibility issues surrounding the implementation of a controlled crossing outside the park entrance (the desire line). There are dropped kerbs for off-street parking in the vicinity and a significant level of on-street parking would need to be removed for visibility. However, in the context of a speed reduction, there are other options potentially available for an uncontrolled crossing. • Casualty data: No incidents reported in the latest 3 year period (up to August 2021). <p>Anticipated Costs: A very high level estimate for an informal crossing and a 20mph zone would be around £40,000. This would increase significantly if a full zebra crossing were to be installed. The cost of the one way plug would also require investigation before the cost could be estimated.</p>
107	Tilehurst	20mph & Pedestrian Crossing	School Road	Outside The Laurels	Concerns raised regarding perceived vehicle speeds and distance to the nearest assisted crossing point. Requested to consider lowering the speed limit and enhanced crossing facility in this location.	<ul style="list-style-type: none"> • Comment: Considering the proximity to the school, we would need to survey pedestrian flows and consider implementing a controlled crossing (e.g. zebra crossing). • Casualty Data: No incidents on School Rd in this area but 1 incident involving a pedestrian (slight) on Corwen Road in the latest 3 year period (up to August 2021). • Anticipated Costs: A high level estimate for a short section of 20mph with cushions would be £20-25,000 but a zebra crossing could be an additional £60,000.
108	Tilehurst	20mph Zone	St Michaels Road	Whole length	Request for a reduced speed limit along this street.	<ul style="list-style-type: none"> • Comment: A speed survey will be necessary to consider suitability and in supporting the consultation with the Police. Officers recommend including side roads in the zone. • Casualty Data: 1 slight incident reported at the junction with the Meadway in the latest 3 year period (up to August 2021) but did not list speeding as a contributing factor. • Anticipated Costs: A high level estimate would be £100,000, including a number of the no-through-roads.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
109	Tilehurst	Prevent one-way contraventions	The Triangle and Walnut Way	Junction with St Michaels Road	Councillor request for investigation into measures to discourage motorists from contravening the one way restriction at this location.	<ul style="list-style-type: none"> • Comment: There is a correctly signed no-entry restriction at the junction with St Michaels Road and it is going to be challenging to find an engineering solution that prevents access for those willingly contravening the restriction. This could be a potential site for future civil enforcement of moving traffic offences, subject to funding. • Casualty Data: No incidents reported in the latest 3 year period (up to August 2021). • Anticipated Costs: Unable to estimate at this time, as it is dependent on a wider piece of work and the types of technology that will be adopted.
110	Tilehurst	Improved pedestrian crossing facilities	Walnut Way	At the junction with Corwen Road	Request via Ward Councillor for a raised island to be installed, in place of the white-painted area at the junction. The width of Walnut Way at this junction makes it difficult for pedestrians to cross.	<ul style="list-style-type: none"> • Comment: It is expected that the installation of an island at this location, particularly of the dimensions required for a pedestrian refuge, will cause vehicle tracking issues for those wishing to turn right. The turn is constricted normally, due to the priority-flow and build-out feature on Corwen Road. The dedicated right turn filter lane would therefore need to be removed. • Casualty Data: No incidents reported in the latest 3 year period (up to end September 2022). • Anticipated Costs: Feasibility would need to be determined first, as wider engineering may be required if it is potentially deliverable.
111	Tilehurst	Pedestrian Crossing	Westwood Road	Junction with School Road	Request received to install improved pedestrian crossing facilities (ideally controlled) near to the roundabout with School Road.	<ul style="list-style-type: none"> • Comment: There are significant feasibility issues for installing a controlled (e.g. zebra) crossing at this location, as once it would be set back from the junction sufficiently to meet visibility requirements, there are dropped crossings / accesses very close together for a considerable stretch of the road. The crossing would be very far away from the desire line. Uncontrolled options such as a raised table could potentially be considered, potentially as part of an area 20mph scheme. • Casualty Data: 1 slight incident at the School Road junction reported in the latest 3 year period (up to August 2021) but it did not list speeding as a causation factor or involve pedestrians. • Anticipated Costs: Unable to estimate at this time, as a scope of works would need to be considered.

Line No.	Ward	Type of Request / Proposal	Street	Location	Details	Officer Comments
112	Whitley	20mph speed limit	Blandford Road	Entire road	Request for a 20mph speed limit along this road due to complaints about vehicles travelling too fast and concerns about safety, as there are schools nearby.	<ul style="list-style-type: none"> • Comment: A speed survey will be necessary to consider suitability and in supporting the consultation with the Police. Officers recommend including nearby roads in the 20 zone to make it an area wide scheme, however, this would significantly increase the costs. • Casualty Data: 3 slight accidents reported at the Blandford Rd/Hartland Rd junction in the in the latest 3 year period (up to August 2021), none where speeding was considered a contributing factor. • Anticipated Costs: A very high level estimate would be around £70,000 for a 20 zone with calming on Blandford Road, however, this would increase significantly if additional roads are included in the zone.
113	Whitley	20mph	Spencer Road	Request related to this street, but a reasonable 'zone' could be created if including Vernon Crescent and the no-through roads that come from each of these.	Concerns raised by resident, regarding speeding along Spencer Road. It was alleged that vehicles are using the street to avoid speed reducing measures on Whitley Wood Lane/Road (humps and buses stopping).	<ul style="list-style-type: none"> • Comment: A speed survey will be necessary to consider suitability and in supporting the consultation with the Police. Physical measures can be placed in a 30mph area, but officers would recommend a 20mph zone for this type of residential street with nearby roads included. • Casualty Data: No accidents reported in this area in the latest 3 year period (up to August 2021). • Anticipated Costs: A very high level estimate would be around £100,000 for an area wide scheme with some traffic calming features.
114	Whitley	20mph	Whitley Wood Lane	Whole length	Request for speed limit to be reduced to 20mph.	<ul style="list-style-type: none"> • Comment: The street has traffic calming (speed cushions), so changes would be the TRO, signing (including removal of old illuminated units that would no longer be required) and installation of repeater markings. If there is a need to increase the size of existing humps then it may cost approximately £4000 per hump. It may also be worth including side roads in the scheme though this would also increase the cost. • Casualty Data: 5 slight and 1 serious incident reported in the latest 3 year period (up to August 2021). None where speeding was listed as a contributing factor. • Anticipated Costs: A high level estimate for just a 20mph scheme without other features would be £25,000.

This table is arranged by Ward (A-Z), then by Street (A-Z)

APPENDIX G – PRIORITISED LIST OF CYCLING AND WALKING MEASURES

Scheme Reference	Route	Section (From)	Section (To)	Description	Criteria															Total Score	
					1	2	3	4	5	6	7	8									
					LTP4 Theme - People and Places	LTP4 Theme - Healthy Lifestyles	LTP4 Theme - Clean and Green	LTP4 Theme - Inclusive Growth	LTP4 Theme - Smart Solutions	Deliverability	PCT flows	Estimated scheme cost									
Strategic Cycle Routes																					
S6	Wokingham Road (S6)	Cemetery Junction	Simons Lane	Physically protect cyclists where possible on 40mph roads, re-allocate road space - lining and carriageway widening, surface improvements, signage, crossing enhancements on side and main roads, junction improvements to cater for cyclists, parking restrictions, drainage in kerbs, cycle enhancements at signal junctions, cycle counters	5 (Excellent Fit)	5	4 (Significant Fit)	4	4 (Significant Fit)	4	4 (Significant Fit)	4	3 (Moderate Fit)	3	3 (Moderate deliverability issues)	3	5 (Highest PCT flows)	5	2 (Moderate cost band 2m to 4.9m)	2	30
S9	A33 (S9)	Mereoak Park and Ride	Bridge Street	Enhance area under IDR, connect shared use facilities, widen foot/cycleway to 3m, links to new developments south of M4, segregate where possible, crossing improvements on side and main roads, cycle priority at junctions, cycle enhancements at signal junctions, cycle counters	5 (Excellent Fit)	5	4 (Significant Fit)	4	4 (Significant Fit)	4	5 (Excellent Fit)	5	3 (Moderate Fit)	3	3 (Moderate deliverability issues)	3	4	4	2 (Moderate cost band 2m to 4.9m)	2	30
S1	Basingstoke Road (S1)	Oracle Roundabout	Whitley Wood Lane/Imperial Way	Re-allocate road space - lining and carriageway widening, crossing enhancements on side and main roads, bus stop bypasses, gridded gully covers, relocate street furniture, signage, cycle enhancements at signal junctions, cycle counters	5 (Excellent Fit)	5	4 (Significant Fit)	4	4 (Significant Fit)	4	4 (Significant Fit)	4	3 (Moderate Fit)	3	3 (Moderate deliverability issues)	3	5 (Highest flows)	5	1 (High cost band 5m to 9m)	1	29

S8	Shinfield Road/Redlands Road (S8)	Black Boy Roundabout	Queens Road	drainage in kerbs, signage, widen footways, lining, enhance cycle facilities at junctions, improve crossing of main and side roads, introduce shared foot/cycleway, cycle enhancements at signal junctions, cycle counters	4 (Significant Fit)	4	4 (Significant Fit)	4	4 (Significant Fit)	4	4 (Significant Fit)	4	3 (Moderate Fit)	3	3 (Moderate deliverability issues)	3	4	4	2 (Moderate cost band 2m to 4.9m)	2	28
S7	London Road/Reading Road (S7)	Forbury Road/Kings Road	Hurricane Way Roundabout	drainage in kerbs, de-clutter streetscape, enhance cycle facilities at junctions, resurface carriageways and footways, remove guard railing, widen footways, re-allocate road space, signage, lining, improve crossings of side and main roads, cycle enhancements at signal junctions, cycle counters	5 (Excellent Fit)	5	4 (Significant Fit)	4	3 (Moderate Fit)	3	3 (Moderate Fit)	3	3 (Moderate Fit)	3	3 (Moderate deliverability issues)	3	4	4	2 (Moderate cost band 2m to 4.9m)	2	27
Page 92 S4	Oxford Road (S4)	Oxford Road/IDR	Pangbourne Station	Physically protect cyclists where possible, segregated routes, re-allocate road space - lining and carriageway widening, resurface carriageway and footway, signage, extend 20mph zone, crossing enhancements on side and main roads, cycle enhancements at signal junctions, cycle counters	4 (Significant Fit)	4	3 (Moderate Fit)	3	4 (Significant Fit)	4	4 (Significant Fit)	4	3 (Moderate Fit)	3	3 (Moderate deliverability issues)	3	4	4	1 (High cost band 5m to 9m)	1	26

Page 93	S5	Bath Road (S5)	The Green	Bath Road/IDR	Physically protect cyclists where possible, segregated routes, re-allocate road space - lining and carriageway widening, surface improvements, signage, crossing enhancements on side and main roads, widen/new ped/cycle bridge, parking restrictions, cycle enhancements at signal junctions, cycle counters	4 (Significant Fit)	4	4 (Significant Fit)	4	4 (Significant Fit)	4	4 (Significant Fit)	4	3 (Moderate Fit)	3	2 (Significant Deliverability Issues)	2	4	4	1 (High cost band 5m to 9m)	1	26
	S3	Peppard Road (S3)	Norman Place/IDR	Borough Boundary	Signage, lining, widen shared foot/cycleways, maintain vegetation, surface improvements, introduce crossings on main roads and enhance crossing of side roads, introduce shared foot/cycleways, provision for cyclists at main junctions, cycle enhancements at signal junctions, cycle counters	4 (Significant Fit)	4	4 (Significant Fit)	4	3 (Moderate Fit)	3	2 (Limited Fit)	2	3 (Moderate Fit)	3	3 (Moderate deliverability issues)	3	3	3	2 (Moderate cost band 2m to 4.9m)	2	24
	S2	Hemdean Road (S2)	Richfield Avenue /Church Street	Gravel Hill	Re-allocate road space - lining and carriageway/footway widening, crossing enhancements on side and main roads, reduce guard railing, car parking restrictions, signage, surface bridleway, cycle enhancements at signal junctions, cycle counters	3 (Moderate Fit)	3	3 (Moderate Fit)	3	3 (Moderate Fit)	3	2 (Limited Fit)	2	3 (Moderate Fit)	3	3 (Moderate deliverability issues)	3	3	3	2 (Moderate cost band 2m to 4.9m)	2	22

Orbital Cycle Routes																					
O2	Inner Distribution Road (O2)	Circular route	Circular route	Crossing enhancements on main and side roads, segregation where possible, shared use where not, surfacing, signage, cycle enhancements at signal junctions, Mini Hollands treatments - further research required	5 (Excellent Fit)	5	4 (Significant Fit)	4	4 (Significant Fit)	4	4 (Significant Fit)	4	2 (Limited Fit)	2	3 (Moderate deliverability issues)	3	4	4	1 (High cost band 5m to 9m)	1	27
O9	(O9)	Hartland Road/Basingstoke Road	Shepherd House Hill Roundabout	Crossing enhancements on main and side roads, segregation where possible, shared use where not, surfacing, signage, cycle enhancements at signal junctions, Mini Hollands treatments - further research required	4 (Significant Fit)	4	3 (Moderate Fit)	3	4 (Significant Fit)	4	3 (Moderate Fit)	3	2 (Limited Fit)	2	3 (Moderate deliverability issues)	3	3	3	2 (Moderate cost band 2m to 4.9m)	2	24
Page 94 O6	(O6)	Beresford Road/Oxford Road	Richfield Avenue/Caversham Bridge	Crossing enhancements on main and side roads, segregation where possible, shared use where not, surfacing, signage, cycle enhancements at signal junctions, Mini Hollands treatments - further research required	3 (Moderate Fit)	3	3 (Moderate Fit)	3	4 (Significant Fit)	4	2 (Limited Fit)	2	2 (Limited Fit)	2	3 (Moderate deliverability issues)	3	3	3	3 (Low cost band 0 to 1.9m)	3	23
O1	Lower Earley Way (O1)	Showcase Roundabout	M4 Junction 11	signage, maintenance of shared foot/cycleway, protection for cyclists on high speed sections of road, upgrade footway to shared use with widening and resurfacing, new foot/cycleway, priority for cyclists at junctions, crossing improvements, cycle enhancements at signal junctions	3 (Moderate Fit)	3	3 (Moderate Fit)	3	3 (Moderate Fit)	3	3 (Moderate Fit)	3	2 (Limited Fit)	2	3 (Moderate deliverability issues)	3	2	2	3 (Low cost band 0 to 1.9m)	3	22

O3	(O3)	Tilehurst Railway Station/Ox ford Road	Bath Road/Ol d Bath Road	Crossing enhancements on main and side roads, segregation where possible, shared use where not, surfacing, signage, cycle enhancements at signal junctions, Mini Hollands treatments - further research required	4 (Significan t Fit)	4	3 (Moderate Fit)	3	3 (Moderate Fit)	3	3 (Moderate Fit)	3	2 (Limited Fit)	2	3 (Moderate deliverability issues)	3	2	2	2 (Moderate cost band 2m to 4.9m)	2	22
O5	(O5)	Berkeley Avenue/B ath Road	London Road/Sil ver Street	Crossing enhancements on main and side roads, segregation where possible, shared use where not, surfacing, signage, cycle enhancements at signal junctions, Mini Hollands treatments - further research required	3 (Moderate Fit)	3	3 (Moderate Fit)	3	3 (Moderate Fit)	3	2 (Limited Fit)	2	2 (Limited Fit)	2	3 (Moderate deliverability issues)	3	3	3	3 (Low cost band 0 to 1.9m)	3	22
Page 95 O10	(O10)	Cutbush Lane/Low er Earley Way	Meadow Road/Wo kingham Road	Crossing enhancements on main and side roads, segregation where possible, shared use where not, surfacing, signage, cycle enhancements at signal junctions, Mini Hollands treatments - further research required	3 (Moderate Fit)	3	3 (Moderate Fit)	3	3 (Moderate Fit)	3	2 (Limited Fit)	2	2 (Limited Fit)	2	3 (Moderate deliverability issues)	3	2	2	3 (Low cost band 0 to 1.9m)	3	21
O11	(O11)	Loddon Bridge Road/Wok ingham Road	Butts Hill Road/We stern Avenue	Crossing enhancements on main and side roads, segregation where possible, shared use where not, surfacing, signage, cycle enhancements at signal junctions, Mini Hollands treatments - further research required	4 (Significan t Fit)	4	3 (Moderate Fit)	3	3 (Moderate Fit)	3	2 (Limited Fit)	2	2 (Limited Fit)	2	3 (Moderate deliverability issues)	3	2	2	2 (Moderate cost band 2m to 4.9m)	2	21

O4	(O4)	Groveland s Road/Oxfo rd Road	Liebenro od Road/Bat h Road	Crossing enhancements on main and side roads, segregation where possible, shared use where not, surfacing, signage, cycle enhancements at signal junctions, Mini Hollands treatments - further research required	3 (Moderate Fit)	3	3 (Moderate Fit)	3	3 (Moderate Fit)	3	2 (Limited Fit)	2	2 (Limited Fit)	2	3 (Moderate deliverability issues)	3	2	2	3 (Low cost band 0 to 1.9m)	3	21
O7	(O7)	Priest Hill/Hemd ean Road	Caversh am Park Road/He nley Road	Crossing enhancements on main and side roads, segregation where possible, shared use where not, surfacing, signage, cycle enhancements at signal junctions, Mini Hollands treatments - further research required	3 (Moderate Fit)	3	3 (Moderate Fit)	3	3 (Moderate Fit)	3	3 (Moderate Fit)	3	2 (Limited Fit)	2	3 (Moderate deliverability issues)	3	2	2	2 (Moderate cost band 2m to 4.9m)	2	21
O8	(O8)	Rose Kiln Lane/A33	Three Tuns	Crossing enhancements on main and side roads, segregation where possible, shared use where not, surfacing, signage, cycle enhancements at signal junctions, Mini Hollands treatments - further research required	3 (Moderate Fit)	3	3 (Moderate Fit)	3	3 (Moderate Fit)	3	3 (Moderate Fit)	3	2 (Limited Fit)	2	3 (Moderate deliverability issues)	3	2	2	2 (Moderate cost band 2m to 4.9m)	2	21
Leisure Cycle Routes																					
L2	(L2)	West of Hanger Road/Stati on Road	Thames Valley Park	signage, annual vegetation maintenance, cycle maintenance points, surfacing, lighting	3 (Moderate Fit)	3	3 (Moderate Fit)	3	3 (Moderate Fit)	3	3 (Moderate Fit)	3	1 (No Fit)	1	4 (Limited deliverability issues)	4	3	3	2 (Moderate cost band 2m to 4.9m)	2	22
L3	(L3)	Rose Kiln Lane/A33	Park Lane	signage, annual vegetation maintenance, cycle maintenance points, surfacing, lighting	2 (Limited Fit)	2	2 (Limited Fit)	2	3 (Moderate Fit)	3	4 (Significant Fit)	4	1 (No Fit)	1	4 (Limited deliverability issues)	4	2	2	3 (Low cost band 0 to 1.9m)	3	21
L5	(L5)	River Kennet/Ri ver Thames	Tilehurst Station	signage, annual vegetation maintenance, cycle maintenance points, surfacing, lighting	3 (Moderate Fit)	3	3 (Moderate Fit)	3	3 (Moderate Fit)	3	2 (Limited Fit)	2	1 (No Fit)	1	4 (Limited deliverability issues)	4	2	2	3 (Low cost band 0 to 1.9m)	3	21

L1	(L1)	Sulham Hill	Nunhide Lane/Pincents Lane	signage, annual vegetation maintenance, cycle maintenance points, surfacing, lighting	2 (Limited Fit)	2	2 (Limited Fit)	2	3 (Moderate Fit)	3	2 (Limited Fit)	2	1 (No Fit)	1	4 (Limited deliverability issues)	4	2	2	3 (Low cost band 0 to 1.9m)	3	19
L4	(L4)	Southcote Farm Lane	Rose Kiln Lane/Mat alan	signage, annual vegetation maintenance, cycle maintenance points, surfacing, lighting	2 (Limited Fit)	2	2 (Limited Fit)	2	2 (Limited Fit)	2	2 (Limited Fit)	2	1 (No Fit)	1	4 (Limited deliverability issues)	4	1 (Insignificant flows)	1	3 (Low cost band 0 to 1.9m)	3	17
Local Cycle Routes																					
LO1	Town Centre (LO1)	n/a	n/a	crossing enhancements, cycle enhancements at signals, cycle counters, signage, allow cycling in new areas, lining, smart secure cycle parking	4 (Significant Fit)	4	3 (Moderate Fit)	3	3 (Moderate Fit)	3	4 (Significant Fit)	4	4 (Significant Fit)	4	3 (Moderate deliverability issues)	3	4	4	3 (Low cost band 0 to 1.9m)	3	28
LO2	North Reading (LO2)	n/a	n/a	signage, speed limit reductions, traffic calming, cycle priority measures, lining, improved and new crossings, cycle enhancements at signals, surface improvements	3 (Moderate Fit)	3	4 (Significant Fit)	4	3 (Moderate Fit)	3	3 (Moderate Fit)	3	2 (Limited Fit)	2	3 (Moderate deliverability issues)	3	3	3	2 (Moderate cost band 2m to 4.9m)	2	23
LO4	South Reading (LO4)	n/a	n/a	signage, speed limit reductions, traffic calming, cycle priority measures, lining, improved and new crossings, cycle enhancements at signals, surface improvements	3 (Moderate Fit)	3	4 (Significant Fit)	4	3 (Moderate Fit)	3	3 (Moderate Fit)	3	2 (Limited Fit)	2	3 (Moderate deliverability issues)	3	3	3	2 (Moderate cost band 2m to 4.9m)	2	23
LO3	East Reading (LO3)	n/a	n/a	signage, speed limit reductions, traffic calming, cycle priority measures, lining, improved and new crossings, cycle enhancements at signals, surface improvements	3 (Moderate Fit)	3	4 (Significant Fit)	4	3 (Moderate Fit)	3	3 (Moderate Fit)	3	2 (Limited Fit)	2	3 (Moderate deliverability issues)	3	3	3	1 (High cost band 5m to 9m)	1	22

LO5	West Reading (LO5)	n/a	n/a	signage, speed limit reductions, traffic calming, cycle priority measures, lining, improved and new crossings, cycle enhancements at signals, surface improvements	3 (Moderate Fit)	3	4 (Significant Fit)	4	3 (Moderate Fit)	3	3 (Moderate Fit)	3	2 (Limited Fit)	2	3 (Moderate deliverability issues)	3	3	3	1 (High cost band 5m to 9m)	1	22
Prestige Walking Routes																					
P2	Station Hill	Queen Victoria Street/Broad Street	Vastern Road	Enhance public realm, reposition street furniture, resurface, signal crossing improvements, enhance uncontrolled crossings	5 (Excellent Fit)	5	3 (Moderate Fit)	3	3 (Moderate Fit)	3	4 (Significant Fit)	4	4 (Significant Fit)	4	4 (Limited deliverability issues)	4	5 (Highest flows)	5	2 (Moderate cost band 2m to 4.9m)	2	30
P1	Broad Street	Kings Street/Broad Street	Oxford Road/Howard Street	Enhance public realm, reposition street furniture, resurface, signal crossing improvements, enhance uncontrolled crossings	5 (Excellent Fit)	5	3 (Moderate Fit)	3	3 (Moderate Fit)	3	3 (Moderate Fit)	3	4 (Significant Fit)	4	4 (Limited deliverability issues)	4	5 (Highest flows)	5	2 (Moderate cost band 2m to 4.9m)	2	29
Primary Walking Routes																					
PM4	Redlands Road	Christchurch Road Local Centre	Duke Street/Broad Street	Signal crossing improvements, relocate street furniture, side road crossing enhancements, resurfacing areas of poor quality, maintain vegetation, signage	4 (Significant Fit)	4	4 (Significant Fit)	4	4 (Significant Fit)	4	3 (Moderate Fit)	3	3 (Moderate Fit)	3	3 (Moderate deliverability issues)	3	4	4	2 (Moderate cost band 2m to 4.9m)	2	27
PM1	Caversham	Oracle Roundabout	Kidmore End Road	Signal crossing improvements, maintain vegetation, relocate street furniture, side road crossing enhancements, resurfacing areas of poor quality, introduce footway on desire line at Peppard Road, signage	4 (Significant Fit)	4	4 (Significant Fit)	4	4 (Significant Fit)	4	3 (Moderate Fit)	3	3 (Moderate Fit)	3	3 (Moderate deliverability issues)	3	3	3	2 (Moderate cost band 2m to 4.9m)	2	26
PM2	Wokingham Road	St Peters Road	Kings Street/Broad Street	Signal crossing improvements, relocate street furniture, side road crossing enhancements, resurfacing areas of poor quality, signage	4 (Significant Fit)	4	4 (Significant Fit)	4	4 (Significant Fit)	4	3 (Moderate Fit)	3	3 (Moderate Fit)	3	3 (Moderate deliverability issues)	3	3	3	2 (Moderate cost band 2m to 4.9m)	2	26

PM3	University of Reading	Christchurch Road Local Centre	Bridge Street/Broad Street	Enhance public realm, signal crossing improvements, relocate street furniture, side road crossing enhancements, resurfacing areas of poor quality, maintain vegetation, signage	4 (Significant Fit)	4	4 (Significant Fit)	4	4 (Significant Fit)	4	3 (Moderate Fit)	3	3 (Moderate Fit)	3	3 (Moderate deliverability issues)	3	3	3	2 (Moderate cost band 2m to 4.9m)	2	26
PM6	Oxford Road	Howard Street	Norcot Road	Signal crossing improvements, relocate or remove street furniture, side road crossing enhancements	4 (Significant Fit)	4	3 (Moderate Fit)	3	3 (Moderate Fit)	3	4 (Significant Fit)	4	3 (Moderate Fit)	3	3 (Moderate deliverability issues)	3	4	4	2 (Moderate cost band 2m to 4.9m)	2	26
PM5	Bath Road	Castle Street	Honey End Lane	Signal crossing improvements, relocate street furniture, side road crossing enhancements, resurfacing areas of poor quality, maintain vegetation, signage	4 (Significant Fit)	4	4 (Significant Fit)	4	4 (Significant Fit)	4	3 (Moderate Fit)	3	3 (Moderate Fit)	3	3 (Moderate deliverability issues)	3	2	2	2 (Moderate cost band 2m to 4.9m)	2	25

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